

# Transport and Environment Committee

10.00am, Tuesday, 12 January 2016

## Leith Programme – Objections to Traffic Regulation Order – Leith Walk (Brunswick Street to Dalmeny Street) and Redetermination Order – Leith Walk (Brunswick Street to Iona Street)

Item number	7.13
Report number	
Executive/routine	Executive
Wards	Leith Walk

### Executive summary

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The Leith Programme consists of approximately £9 million of road, footway and cycle improvements along the whole length of Constitution Street and Leith Walk, which will transform the nature and operation of these streets. The Programme is being delivered in a number of phases over several financial years.

The next phase of the Programme to be implemented (Phase Four) will be the section of Leith Walk between Brunswick Street and Iona Street. The proposals for this section require both a Traffic Regulation Order and a Redetermination Order. This report details the results of the statutory consultation for both Orders.

Four objections to the advertised Traffic Regulation Order were received and two objections were received to the Redetermination Order. These objections and the Council's responses are detailed in this report.

Plans showing the proposed road layouts are appended to this report.

### Links

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Coalition pledges	<a href="#">P44</a> , <a href="#">P45</a>
Council outcomes	<a href="#">CO19</a> , <a href="#">CO22</a>
Single Outcome Agreement	<a href="#">SO4</a>

## Leith Programme – Objections to Traffic Regulation Order – Leith Walk (Brunswick Street to Dalmeny Street) and Redetermination Order – Leith Walk (Brunswick Street to Iona Street)

### Recommendations

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- 1.1 It is recommended that the Committee:
  - 1.1.1 notes the objections received to the advertised Traffic Regulation Order and Redetermination Order and the Council's comments in response;
  - 1.1.2 approves the proposed changes to the advertised Traffic Regulation Order detailed in this report, relating to parking and loading restrictions in Pilrig Street;
  - 1.1.3 sets aside the objections received to the Traffic Regulation Order; and
  - 1.1.4 instructs officials to refer the objections to the Redetermination Order to Scottish Ministers.

### Background

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- 2.1 The Leith Programme consists of approximately £9 million of road, footway and cycle improvements along the whole length of Constitution Street and Leith Walk, which will transform the nature and operation of these streets. The Programme is being delivered in a number of phases over several financial years.
- 2.2 Works to Constitution Street were completed in November 2013 and improvements to the section of Leith Walk between Crown Place and Iona Street were completed in December 2014. The third phase of the Programme, which involved upgrading the Foot of the Walk junction, was completed in June 2015.

### Main report

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- 3.1 The next phase of the Programme to be implemented (Phase Four) will be the section of Leith Walk between Brunswick Street and Iona Street. An extensive programme of improvements is proposed, including:
  - upgrades to the signal-controlled junctions at McDonald Road and Pilrig Street;

- the introduction of a prohibition on entry to Iona Street from Leith Walk, except for cyclists;
- alterations to parking and loading facilities;
- alterations to bus lane operating hours;
- provision of segregated cycling facilities in each direction;
- relocation of a bus stop from Leith Walk onto Pilrig Street;
- re-laying footways with flag paving;
- reducing road widths and providing raised tables at side streets;
- installing a new pedestrian refuge island;
- resurfacing road surfaces to benefit all road users;
- removing redundant street furniture, reducing street clutter and providing planters to create a more attractive environment; and
- relocating domestic waste containers into dedicated bays.

3.2 There is insufficient road space to achieve all of the above, and to maintain an acceptable level of public transport priority on Leith Walk, while retaining the existing dedicated lay-bys for loading and parking bays. It is therefore proposed to provide a road layout where the two kerbside traffic lanes will function as bus lanes during the peak traffic periods on Mondays to Fridays and provide facilities for loading and parking at other times.

### **Changes to Loading and Parking Provision**

- 3.3 A review of all existing loading and parking facilities along this section of Leith Walk has been undertaken and a number of changes are proposed in order to facilitate the improvements.
- 3.4 The Council appreciates that it is important for businesses and residents to have access to loading facilities, and has sought to ensure that these are provided at the most suitable locations to meet local demand. In order to inform the design, a survey was undertaken to determine the loading needs of businesses within this section of Leith Walk.
- 3.5 Plans showing the location of proposed loading and parking bays under the new layout, and their associated hours of operation, are provided in Appendix 1.

### **Leith Walk**

3.6 It is proposed to increase the number of loading bays on this section of Leith Walk from 18 spaces to 27 spaces. To achieve this, and to make other proposed changes to the road layout, the number of short stay parking bays on this section of Leith Walk would be reduced from 32 spaces to nine spaces.

3.7 In order to maintain public transport priority during peak traffic periods, the loading and parking bays that will be provided on this section of Leith Walk will not operate from 7.30am–9.30am or 4.00pm–6.30pm on Mondays to Fridays. This is consistent with other main routes into the city at peak times.

**Albert Street**

3.8 It is also proposed to convert 22 uncontrolled parking spaces on Albert Street into three loading spaces and 19 short stay parking spaces. This will increase the turnover of these spaces, to the benefit of local businesses and customers looking for short stay parking. The one existing disabled parking space in this area will be retained.

3.9 Further details of the proposed changes to loading and parking facilities are provided in Tables 1 and 2 below:

Type	Location	Operation	Spaces
Existing	Leith Walk	Mon - Fri: 7.30am – 6.30pm, Sat: 8.30am – 6.30pm	18
Proposed	Leith Walk	Mon - Fri: 9.30am – 4.00pm, Sat: 8.30am – 6.30pm	27
	Albert Street	Mon - Fri: 7.30am – 6.30pm	3

Table 1 – Proposed changes to loading bays.

Type	Location	Operation	Spaces
Existing	Leith Walk	Mon - Fri: 7.30am – 6.30pm, Sat: 8.30am – 6.30pm 60 mins max stay, no return within 90 mins	32
	Albert Street	Uncontrolled	22
	Albert Street	Disabled	1
Proposed	Leith Walk	Mon - Fri: 9.30am – 4.00pm, Sat: 8.30am – 6.30pm 60 mins max stay, no return within 90 mins	9
	Albert Street	Mon - Fri: 7.30am – 6.30pm 60 mins max stay, no return within 90 mins	19
	Albert Street	Disabled	1

Table 2 – Proposed changes to parking bays

## Provision of Segregated Cycling Facilities

3.10 New one-way segregated cycle facilities are proposed in each direction on Leith Walk between McDonald Road and Pilrig Street. These facilities will generally be 1.5m wide and will be physically segregated from traffic by a 0.5m wide segregation zone.

## Changes to Bus Lanes and Bus Stops

3.11 In order to maintain off-peak loading and parking provision for local residents and businesses, it is proposed to alter the operating hours of the bus lanes on this section of Leith Walk as shown in Table 3 below:

Existing Operating Hours	Proposed Operating Hours
Mon - Fri: 7.30am to 6.30pm, Sat: 8.30am to 6.30pm* *temporarily changed for city-wide trial	Mon - Fri: 7.30am to 9.30am and 4.00pm to 6.30pm

Table 3 – Proposed changes to bus lane operating hours

3.12 These revised bus lane operating hours will ensure that public transport priority is provided during the peak traffic periods on Leith Walk, while allowing loading and parking facilities to be provided within the same areas of road space at other times.

3.13 For reasons of consistency, it will also be necessary to alter the operating hours of the existing section of southbound bus lane on Leith Walk, between Dalmeny Street and Iona Street, to the proposed operating hours shown in Table 3 above.

3.14 These changes to the bus lane operating hours on this section of Leith Walk are integral to the proposed operation of the new road layout, with areas of road space functioning both as bus lanes and as facilities for loading and parking at different times of day. As such, these changes are being pursued independently of the current trial to change all day bus lanes throughout the remainder of the city into peak periods bus lanes, on an experimental basis.

3.15 It is also proposed to relocate the existing northbound bus stop outside No 378 Leith Walk into Pilrig Street (west of Spey Street). This will remove the existing potential for conflict between buses using the stop and cyclists and other traffic approaching the junction at Pilrig Street. It will also allow pedestrian facilities to be improved at this location. This stop is used only by the Lothian Buses No. 11 service.

- 3.16 In order to ensure two-way traffic flow past the new position of the stop, approximately 30m of new double yellow line waiting and loading restrictions were proposed on the north side of Pilrig Street, between Pilrig Glebe and Arthur Street. After consideration of the objections received to the advertised Traffic Regulation Order, it is now proposed to install single yellow lines over part of this length instead. Further details are provided in paragraphs 3.54 to 3.59 of this report.
- 3.17 Lothian Buses has been consulted over the proposed changes to bus lane operating hours and the relocation of the bus stop, and is satisfied with the proposals.

### **Prohibited Entry**

- 3.18 As part of the proposals, it is proposed to prohibit entry for vehicles into Iona Street from Leith Walk. This will improve traffic flow in the area around the Pilrig Street and Iona Street junctions, mainly by preventing queues of traffic waiting to turn right into Iona Street from obstructing the Leith Walk/Pilrig Street junction.
- 3.19 The prohibition of this right turn was suggested by Leith Central Community Council during an earlier Leith Programme consultation. However, it will be possible to provide a clearer, and therefore more self-enforcing, road layout by prohibiting entry for all vehicles rather than by only prohibiting right turn manoeuvres.
- 3.20 Traffic counts have been undertaken in order to establish the number of vehicles that currently enter Iona Street from Leith Walk and it is not considered likely that there will be an unacceptable impact from displaced traffic on the various possible alternative routes.
- 3.21 In line with commitments in the Council's Active Travel Action Plan, cyclists will be exempted from the proposed prohibition on entry. A contraflow cycle lane will be provided to allow cycle access into Iona Street from Leith Walk.
- 3.22 The exit for vehicles from Iona Street onto Leith Walk will be retained under the proposed new layout.

### **Relocation of Waste Containers into Dedicated Bays**

- 3.23 Dedicated areas of road space will be created to accommodate domestic waste containers. This will ensure that these containers are removed from the footway, creating a more attractive environment for pedestrians. There will also be no conflict with cyclists using the segregated cycle lanes, particularly when refuse is being collected.

## **Changes to Disabled Parking Bays**

- 3.24 In order to provide dedicated loading facilities on the northbound side of Leith Walk at Albert Place, it is proposed to remove two existing disabled parking bays. It is considered that these bays are no longer required, as they were originally introduced to assist visitors to the former Council Social Work Department office at Shrubhill House. This building has now been demolished and a new development, consisting of student accommodation above ground floor retail premises, is currently under construction.
- 3.25 The existing disabled parking bay at the west end of Albert Street and those outside McDonald Road Library will be retained under the proposed design.

## **Future Proofing**

- 3.26 The proposed design for this section of Leith Walk has been developed in consultation with the Council's Tram Team and the Tram Operator.
- 3.27 The proposed design is compatible with a future tram extension on this section of Leith Walk, and significant physical changes would not be required to kerblines to accommodate this.

## **Statutory Consultation**

- 3.28 In line with the statutory requirements for consultations being carried out under the terms of the Road Traffic Regulation Act 1984, the draft Traffic Regulation Order was advertised between 20 October and 17 November 2015.
- 3.29 In line with the statutory requirements for consultations being carried out under the terms of the Roads (Scotland) Act 1984, the draft Redetermination Order was advertised during the same period.
- 3.30 In addition, approximately 2,000 letters were delivered to businesses and residents on Leith Walk and streets surrounding the area covered by the Orders.
- 3.31 Four objections were received to the advertised Traffic Regulation Order, and two objections were received to the advertised Redetermination Order. The objections received are listed in Appendix 3.
- 3.32 Six further responses were also received during the statutory consultation period. Four of these responses included comments or questions relating to the advertised Orders, while two expressions of support for the proposals were also received, including one from SPOKES.
- 3.33 The issues raised within the objections to the Orders relate to:
- the proposed parking restrictions on Albert Street;
  - the proposed 'no entry' restriction for vehicles into Iona Street from Leith Walk;
  - the proposed changes to the Leith Walk/Pilrig Street junction, and

- the proposed relocation of the bus stop from outside No. 378 Leith Walk to Pilrig Street, and proposed waiting restrictions associated with the relocation

3.34 Further details of the objections received and the Council's responses are provided in Appendix 4.

Parking restrictions on Albert Street

3.35 One objector raised concerns that the introduction of short stay parking bays at the west end of Albert Street will have an impact on the remaining uncontrolled spaces on the street as there will be fewer spaces for those wishing to park longer than the permitted maximum stay, increasing existing parking pressure in the street further.

3.36 The proposed short stay bays can be used up to a maximum of 60 minutes between 7.30am and 6.30pm (Monday to Friday). There is no maximum length of stay at all other times. This means that residents will be able to use these bays for evening and overnight parking between 5.30pm and 8.30am on weekdays and at all times at weekends.

3.37 The short stay bays are proposed to offset the loss of peak period parking and loading facilities and short stay parking bays on Leith Walk. The 60 minute limit will increase the turnover of these spaces to the benefit of local businesses and customers looking for short stay parking. The bays will also be consistent with the 60 minute maximum stay in force for parking bays on Leith Walk on Mondays to Saturdays.

'No entry' into Iona Street for vehicles

3.38 Two objections were received in relation to the proposal to prohibit access for vehicles into Iona Street from Leith Walk under the new layout.

3.39 The first of these objections concerned a potential increase in traffic levels on Albert Street as an alternative route for eastbound traffic and advised that there are already issues with traffic flow on this street, mainly as a result of double parked vehicles. As such, any additional traffic on Albert Street would exacerbate existing traffic flow issues further.

3.40 Traffic counts were carried out as part of the design process to establish the number of vehicles currently entering Iona Street from Leith Walk. While restricting access to Iona Street will result in additional traffic using adjoining streets, this traffic is likely to be displaced onto a number of possible alternative routes, some nearby and some further afield as part of a more strategic re-routing. As such, the Council does not expect displacement of traffic to cause a significant problem on any individual alternative route.



- 3.41 The second objection received to the proposal to prohibit access to Iona Street from Leith Walk was received from a local business situated on Iona Street. This business operates a builders yard which receives a large number of deliveries made by a range of vehicles, including large articulated vehicles.
- 3.42 The concerns raised within this objection are:
- significant restriction and disruption of access to the business, resulting in a loss of trade;
  - safety concerns around increased HGV traffic travelling past the play park on Iona Street;
  - safety concerns around the physical ability of heavy goods vehicles entering and exiting Iona Street at Easter Road, and
  - impacts in terms of noise, road condition and vehicle maintenance as a result of having to travel over traffic calming features on Iona Street.
- 3.43 The proposed changes would not prevent vehicles leaving the yard from joining Leith Walk, while traffic entering or exiting the yard to/from Easter Road would also be unaffected. As such, three of the four currently possible movements to or from the business would be unaffected by the proposed changes.
- 3.44 The objector also raised concerns over the ability of heavy goods vehicles to make the turn into Iona Street from Easter Road, which would be the likely alternative route to the business for such vehicles. However, swept-path analyses have been reviewed, and there are no particular concerns.
- 3.45 The other issues which are raised within the objection have also been fully considered. The traffic calming measures which are in place on Iona Street are standard features which are common across the city and should not cause any issues for vehicles, including large goods vehicles. With regard to the play park on Iona Street, this is securely fenced off and is remote from the carriageway. While the access to the play park is not gained directly from Iona Street.
- 3.46 Although the issues raised in this objection relate entirely to traffic restrictions proposed as part of the Traffic Regulation Order, the objector also refers to the Redetermination Order within their objection. This objection has therefore been considered to be an objection to the Redetermination Order, in addition to the Traffic Regulation Order.

#### Changes at the Leith Walk/Pilrig Street junction

- 3.47 The final objection was received from a household on Pilrig Street which highlighted a number of concerns relating to the proposed changes at the Leith Walk/Pilrig Street junction.

- 3.48 The proposed changes at the junction reflect the principles of the Scottish Government's 'Designing Streets' guidance and are in keeping with the design principles for the Leith Programme as a whole, which were determined following extensive consultation with local stakeholders. These principles are consistent for all phases of the Programme, and promote simplified road layouts which are attractive for pedestrians and cyclists.
- 3.49 The objector believes that the reduction in northbound traffic lanes on approach to the junction and changes to the pedestrian crossing phasing will increase congestion and potentially lead to traffic 'rat-running' through Spey Terrace and Dryden Street to avoid the junction.
- 3.50 Under the proposals, to facilitate improvements to pedestrian and cycling facilities the northbound approach would be reduced from two traffic lanes at present to one lane, and the pedestrian crossing phasing at the junction would also be changed. There would be significant benefit for pedestrians as a result of widened footways and a single stage crossing on all legs of the junction. While journey times for vehicles are likely to increase, it is not considered that any unacceptable impacts will arise from the proposed changes in terms of congestion or that traffic will be displaced onto alternative routes.
- 3.51 The objector also raised concerns over the alignment of the northbound cycle lane at the junction, as a significant volume of traffic turns left at the junction into Pilrig Street and will be required to cross the lane. However, this is a standard junction layout and no issues are anticipated. SPOKES has been consulted on the proposals and have expressed support for the scheme in general, and has raised no concerns in relation to the proposed junction layout.
- 3.52 Under the existing layout, cyclists are not only in conflict with vehicles crossing the cycle lane to reach the left-turn lane, but also with the bus stop and loading bay on the northbound approach to the junction. The proposed new layout would remove all conflicts with the bus stop at this location, and would reduce conflicts between vehicles using the loading bay, cyclists and left-turning traffic. Under the new layout, the issue with cyclists and left-turning traffic would be confined to a specific location rather than over a greater length as at present.
- 3.53 As elements of the objection concerning changes at the Pilrig Street junction relate to the proposal to change areas of existing carriageway to footway, this has been considered as an objection to the Redetermination Order in addition to the Traffic Regulation Order.

Relocation of bus stop from Leith Walk to Pilrig Street (and associated waiting restrictions)

- 3.54 The above objector also raised concerns about the proposed relocation of the northbound bus stop from outside No 378 Leith Walk to Pilrig Street. The objector states that the stop will no longer serve the shops on Leith Walk, and that the footway at the proposed location is already narrow and the stop will act as a further obstruction for pedestrians.
- 3.55 The objection also states that the proposed 24 hour waiting and loading restrictions on Pilrig Street in the vicinity of the stop are unnecessary, and that any vehicles parked overnight would not cause any traffic flow issues.
- 3.56 Relocating the bus stop from Leith Walk will remove potential for conflict between buses using the stop and cyclists and other northbound traffic approaching the junction at Pilrig Street. It will allow pedestrian and cycling facilities to be improved at this location.
- 3.57 The stop is only used by one service (Lothian Buses No 11), and Lothian Buses has been consulted over the proposed relocation and have no objections.
- 3.58 The changes to waiting and loading restrictions on Pilrig Street are proposed in order to ensure two way traffic flow past the new position of the stop. The main concern is that, if two way traffic flow past the new stop is not possible due to parked vehicles, that westbound traffic will queue back to Leith Walk when a bus is stationary at the stop. However, it is accepted that this is unlikely to occur in the evenings or on Sundays. It is therefore proposed to change the double yellow line restrictions on the north side of Pilrig Street between Pilrig Glebe and Arthur Street to a single yellow line restriction (Monday to Saturday). This will enable parking and loading on the single yellow line overnight and on Sundays.
- 3.59 The number of passengers picked up by buses at the stop is very low, with the stop operating mainly as a drop off point. As such, no bus shelter is required on the footway, and there are expected to be extremely few occasions where waiting passengers may cause an obstruction. It is also proposed to widen the footway slightly to 1.8 metres. However, it is not possible to increase this further due to width restraints on Pilrig Street. A temporary bus stop was also introduced at this location during previous tram works on Leith Walk, and this operated satisfactorily.
- 3.60 Those who had objected to the advertised Orders were contacted with a detailed response to their objections, including details of the proposed amendments to the design. To date, no individuals have withdrawn their objection.

## **Next Steps**

- 3.61 It is recommended that the Committee approves the changes to the advertised Traffic Regulation Order relating to parking and loading restrictions in Pilrig Street, and also set aside the maintained objections to the Traffic Regulation Order to enable the Order to be made.
- 3.62 In accordance with the requirements of the Roads (Scotland) Act 1984, all objections to a Redetermination Order must be referred to Scottish Ministers. It is therefore recommended that the Committee instructs Officers to refer to Scottish Ministers the two objections which were received to the Redetermination Order. The process that Scottish Ministers use to reach their determination on the Order is at their discretion. They may decide to hold a public hearing to consider the objections but this is not a mandatory requirement.

## **Measures of success**

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- 4.1 The measure of success for the Leith Programme will be an improved, more attractive environment along the Leith Walk and Constitution Street corridors, particularly for pedestrians and cyclists.

## **Financial impact**

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- 5.1 The costs associated with the statutory procedures to make the necessary Traffic Regulation Orders and Redetermination Order are estimated at £5,000.
- 5.2 The costs for this phase of construction will be subject to the outcome of a competitive tendering process. Construction costs will be fully contained within the Services for Communities managed Capital Investment Programme, and the scheme is supplemented by a significant external funding award from the Scottish Government.

## **Risk, policy, compliance and governance impact**

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- 6.1 The Council's Tram Team and the Tram Operator have been consulted on the design in order to ensure that a tram extension could be delivered on this section of Leith Walk without having to make significant physical changes to the proposed layout.

## Equalities impact

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- 7.1 An Equalities and Rights Impact Assessment (ERIA) for the full Leith Programme commenced during the consultation stage of the scheme and will be in effect throughout the delivery of the project.
- 7.2 It is likely that improvements to footways and pedestrian crossing facilities will have a positive impact on the safety, freedom of movement and access for all who live in or use this section of Leith Walk. This takes into account many people whose characteristics are protected under the Equalities Act 2010.
- 7.3 It is proposed to remove two existing disabled parking bays on Leith Walk, however it is understood that these bays were originally introduced to cater for visitors to the Council's Social Work Department office at Shrubhill House, which is no longer operational.
- 7.4 As such, no adverse impact for mobility-impaired street users is anticipated, and the disabled parking bay at Albert Street and those at McDonald Road Library will remain in place under the proposed new layout.

## Sustainability impact

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- 8.1 The impacts of this report in relation to the three elements of the Climate Change (Scotland) Act 2009 Public Bodies Duties have been considered, and the outcomes are summarised below. Relevant Council sustainable development policies have been taken into account and are noted at Background Reading later in the report.
- 8.2 The proposals set out in this report will reduce carbon emissions by contributing towards the core objectives of the Council's Active Travel Action Plan to increase the number of people walking and cycling in Edinburgh.
- 8.3 The proposals set out in this report will increase the city's resilience to climate change impacts by providing more opportunities for sustainable travel through improvements to walking and cycling infrastructure.
- 8.4 The proposals in this report will help achieve a sustainable Edinburgh by delivering environmental improvements which will benefit all users of Leith Walk.

## Consultation and engagement

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- 9.1 Consultation was carried out between 20 October and 17 November 2015 as part of the statutory process for both Orders. This gave any interested parties the opportunity to submit formally any comments or objections to the Council.
- 9.2 In addition, approximately 2,000 letters were delivered to businesses and residents on Leith Walk and streets surrounding the area covered by the Orders.

- 9.3 The Council's Tram Team and the Tram Operator have been consulted on the design in order to ensure that the tram extension could be delivered on this section of Leith Walk without having to make significant physical changes to the proposed layout.
- 9.4 Lothian Buses has been consulted on the proposed layout, including changes to bus lane operating hours and the relocation of a bus stop from Leith Walk to Pilrig Street.
- 9.5 As part of the wider Leith Programme, extensive consultation has been undertaken for the project with a wide range of stakeholders, with a dedicated webpage set up and regularly updated to provide information on the proposals. Neighbourhood Partnerships, local Ward members, Community Councils, cycling organisations, Lothian Buses and other community groups have all been consulted on the wider proposals.
- 9.6 In addition, regular Key Stakeholder Group meetings are ongoing, while Elected Member Oversight Group meetings are also held at key stages of the project. Members were briefed on the contents of this report at the Oversight Group meeting on 14 December 2015.

## **Background reading/external references**

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The Leith Programme, Consultation and Design, Report to the Transport and Environment Committee by Director of Services for Communities, 19 March 2013.

Active Travel Action Plan

[http://www.edinburgh.gov.uk/info/20037/policies\\_plans\\_and\\_strategies/341/transport\\_policy](http://www.edinburgh.gov.uk/info/20037/policies_plans_and_strategies/341/transport_policy)

### **Paul Lawrence**

Executive Director of Place

Contact: Callum Smith, Senior Professional Officer, Projects Development

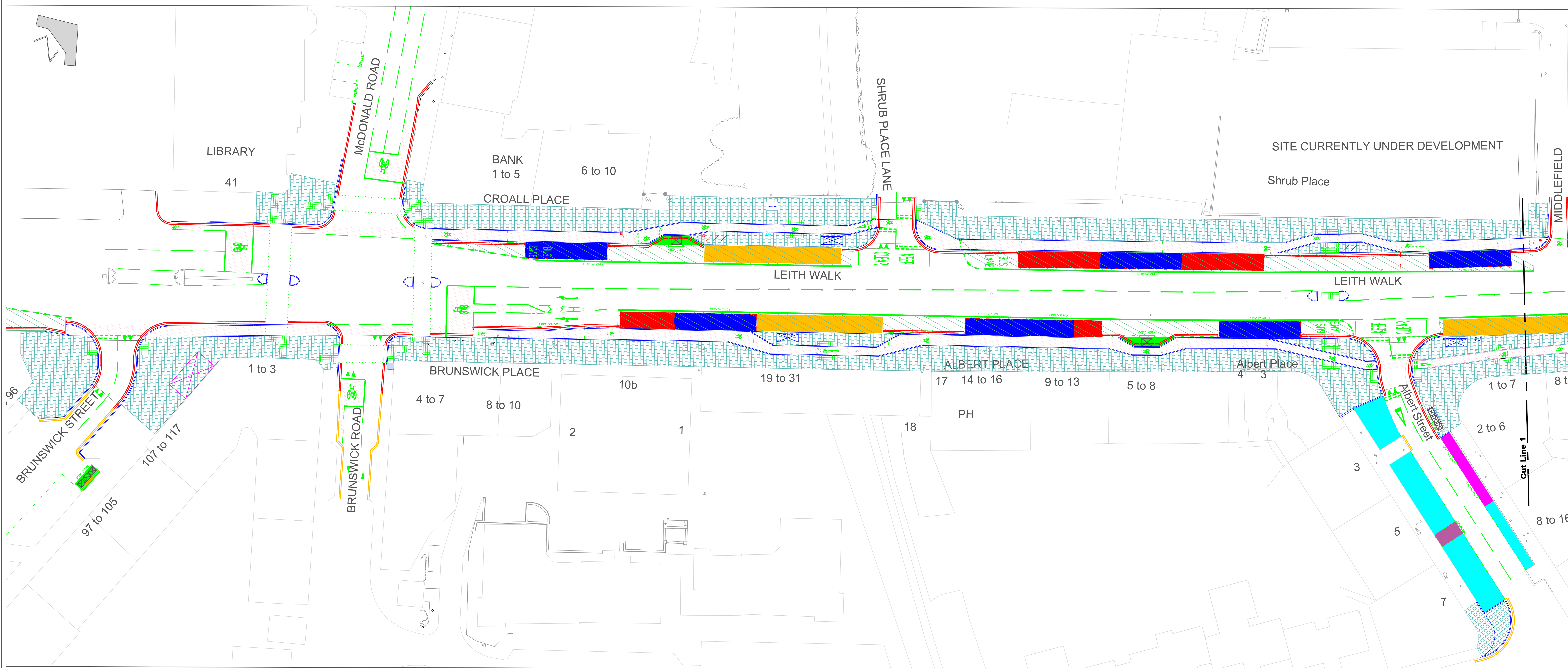
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## Links

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<b>Coalition pledges</b>	P44 – Prioritise keeping our streets clean and attractive P45 – Spend 5% of the transport budget on provision for cyclists
<b>Council outcomes</b>	CO19 – Attractive Places and Well-Maintained – Edinburgh remains an attractive city through the development of high quality buildings and places and the delivery of high standards and maintenance of infrastructure and public realm CO22 – Moving Efficiently – Edinburgh has a transport system that improves connectivity and is green, healthy and accessible.
<b>Single Outcome Agreement</b>	SO4 – Edinburgh’s communities are safer and have improved physical and social fabric
<b>Appendices</b>	Appendix 1 – Plans of the Leith Programme proposals for Leith Walk, between Brunswick Street and Dalmeny Street (Traffic Regulation Order), including proposed amendment to waiting and loading restrictions on Pilrig Street Appendix 2 – Plans of the Leith Programme proposals for Leith Walk between Brunswick Street and Iona Street (Redetermination Order) Appendix 3 – Objections received Appendix 4 – Council’s response letters to objectors





**Bus Lane Operating Hours**

Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
No traffic except buses, taxis and cycles

Other times  
No restrictions

**Parking Bay Restrictions**

Mon - Fri 7.30am - 6.30pm  
60 minutes maximum stay, no return within 90 minutes  
No maximum stay for blue badge holders  
Loading permitted

Other times  
No restrictions

**Loading Bay Restrictions**

Mon - Fri 7.30am - 6.30pm  
No stopping except for loading or blue badge holders  
60 minutes maximum stay, no return within 90 minutes for blue badge holders

Other times  
No restrictions

**Greenway Parking Bay Restrictions**

Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
No Stopping

Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
60 minutes maximum stay, no return within 90 minutes  
No maximum stay for blue badge holders  
Loading permitted

Other times  
No restrictions

**Greenway Loading Bay Restrictions**

Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
No Stopping

Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
No stopping except for loading or blue badge holders  
60 minutes maximum stay, no return within 90 minutes for blue badge holders

Other times  
No restrictions

**Legend**

-  Bus Stop
-  Greenway Parking Bay
-  Greenway Loading Bay
-  Disabled Parking Bay
-  Parking Bay
-  Loading Bay
-  Domestic Bin Bay
-  Bus Lane
-  Advisory Cycle Lane
-  White Road Markings
-  Double Red Road Markings - No stopping
-  Double Yellow Road Markings - No Waiting or Loading

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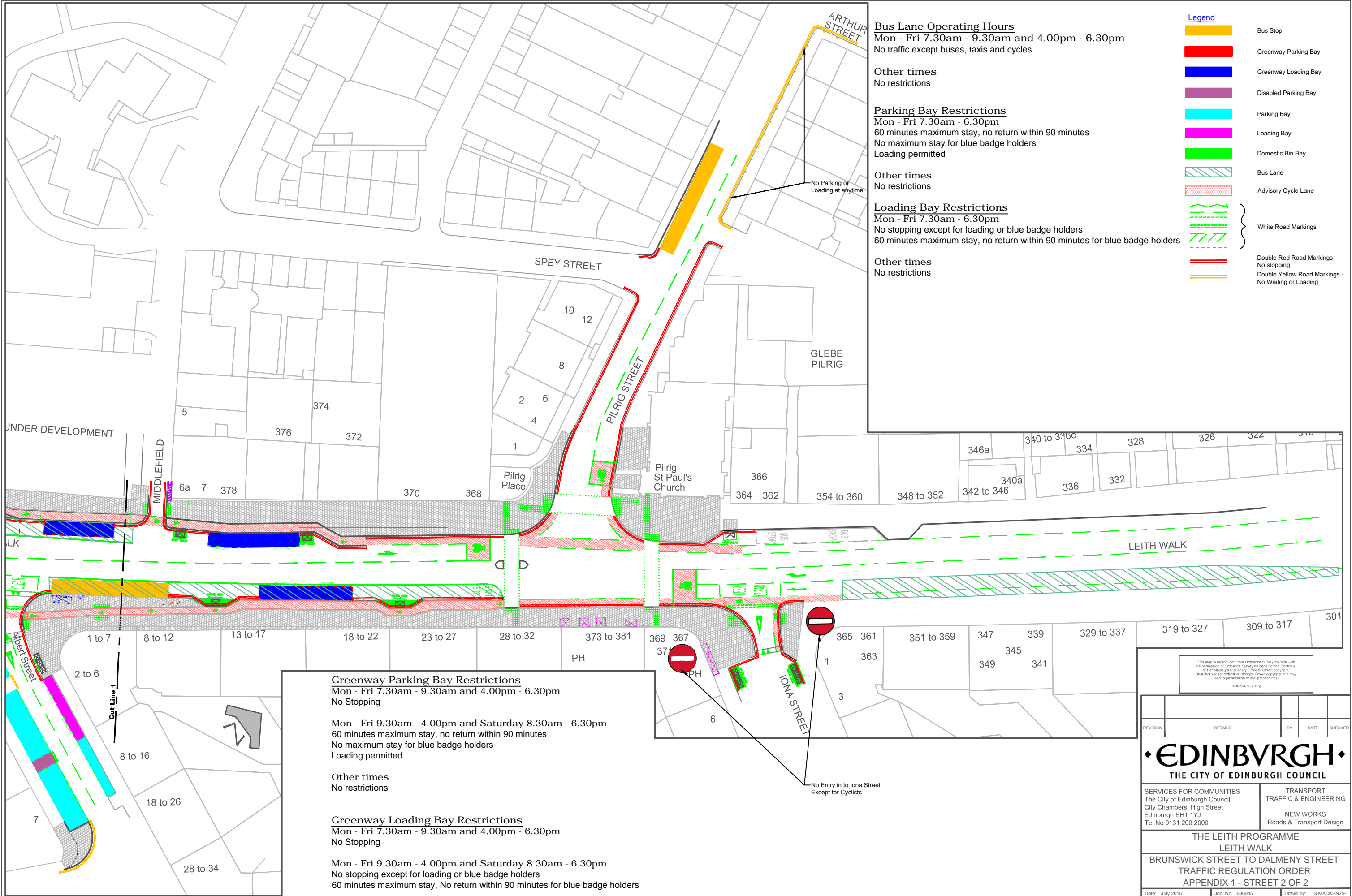
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City Chambers, High Street  
Edinburgh EH1 1YJ  
Tel No 0131 200 2000

TRANSPORT  
TRAFFIC & ENGINEERING  
NEW WORKS  
Roads & Transport Design

THE LEITH PROGRAMME  
LEITH WALK  
BRUNSWICK STREET TO DALMENY STREET  
TRAFFIC REGULATION ORDER  
APPENDIX 1 - STREET 1 OF 2

Date: July 2015 Job No. 636045 Drawn by: S MACKENZIE  
Scale: 1:250 Checked by: D WILSON





**Bus Lane Operating Hours**  
 Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
 No traffic except buses, taxis and cycles

**Other times**  
 No restrictions

**Parking Bay Restrictions**  
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 60 minutes maximum stay, no return within 90 minutes  
 No maximum stay for blue badge holders  
 Loading permitted

**Other times**  
 No restrictions

**Loading Bay Restrictions**  
 Mon - Fri 7.30am - 6.30pm  
 No stopping except for loading or blue badge holders  
 60 minutes maximum stay, no return within 90 minutes for blue badge holders

**Other times**  
 No restrictions

**Legend**

- Bus Stop
- Greenway Parking Bay
- Greenway Loading Bay
- Disabled Parking Bay
- Parking Bay
- Loading Bay
- Domestic Bin Bay
- Bus Lane
- Advisory Cycle Lane
- White Road Markings
- Double Red Road Markings - No stopping
- Double Yellow Road Markings - No Waiting or Loading

**Greenway Parking Bay Restrictions**  
 Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
 No Stopping

Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
 60 minutes maximum stay, no return within 90 minutes  
 No maximum stay for blue badge holders  
 Loading permitted

**Other times**  
 No restrictions

**Greenway Loading Bay Restrictions**  
 Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
 No Stopping

Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
 No stopping except for loading or blue badge holders  
 60 minutes maximum stay, No return within 90 minutes for blue badge holders

**Other times**  
 No restrictions

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REVISION	DETAILS	BY	DATE	CHECKED

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 THE CITY OF EDINBURGH COUNCIL

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 The City of Edinburgh Council  
 City Chambers, High Street  
 Edinburgh EH1 1YJ  
 Tel No 0131 200 2000

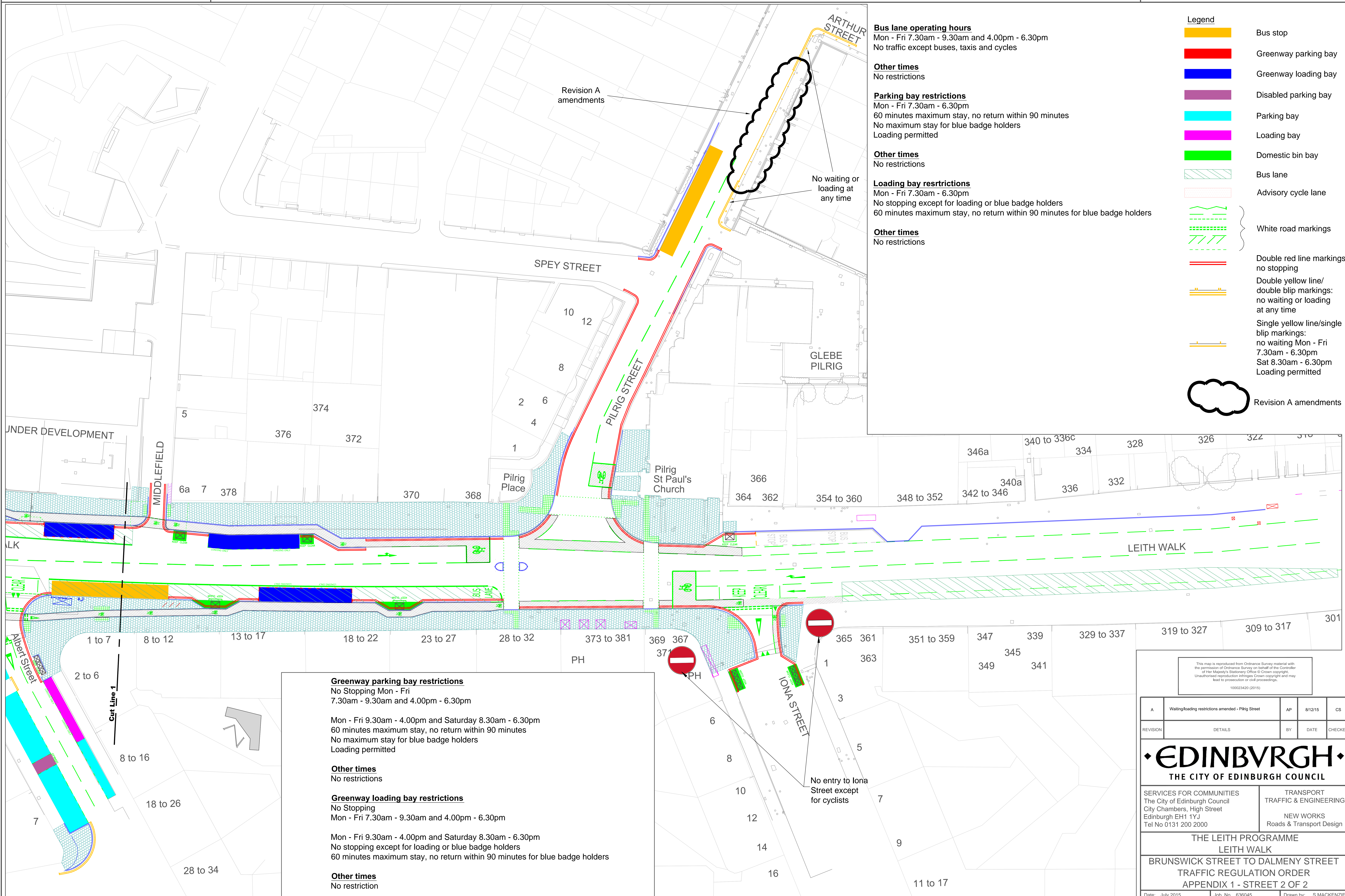
TRANSPORT  
 TRAFFIC & ENGINEERING  
 NEW WORKS  
 Roads & Transport Design

THE LEITH PROGRAMME  
 LEITH WALK  
 BRUNSWICK STREET TO DALMENY STREET  
 TRAFFIC REGULATION ORDER  
 APPENDIX 1 - STREET 2 OF 2

Date: July 2015    Job No. 636045    Drawn by: S MACKENZIE  
 Scale: 1:250    Checked by: D WILSON

DRG. NO. RTD/635045/TRO/402





**Bus lane operating hours**  
 Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm  
 No traffic except buses, taxis and cycles

**Other times**  
 No restrictions

**Parking bay restrictions**  
 Mon - Fri 7.30am - 6.30pm  
 60 minutes maximum stay, no return within 90 minutes  
 No maximum stay for blue badge holders  
 Loading permitted

**Other times**  
 No restrictions

**Loading bay restrictions**  
 Mon - Fri 7.30am - 6.30pm  
 No stopping except for loading or blue badge holders  
 60 minutes maximum stay, no return within 90 minutes for blue badge holders

**Other times**  
 No restrictions

**Legend**

- Bus stop
- Greenway parking bay
- Greenway loading bay
- Disabled parking bay
- Parking bay
- Loading bay
- Domestic bin bay
- Bus lane
- Advisory cycle lane
- White road markings
- Double red line markings: no stopping
- Double yellow line/ double blip markings: no waiting or loading at any time
- Single yellow line/single blip markings: no waiting Mon - Fri 7.30am - 6.30pm Sat 8.30am - 6.30pm Loading permitted
- Revision A amendments

**Greenway parking bay restrictions**  
 No Stopping Mon - Fri  
 7.30am - 9.30am and 4.00pm - 6.30pm

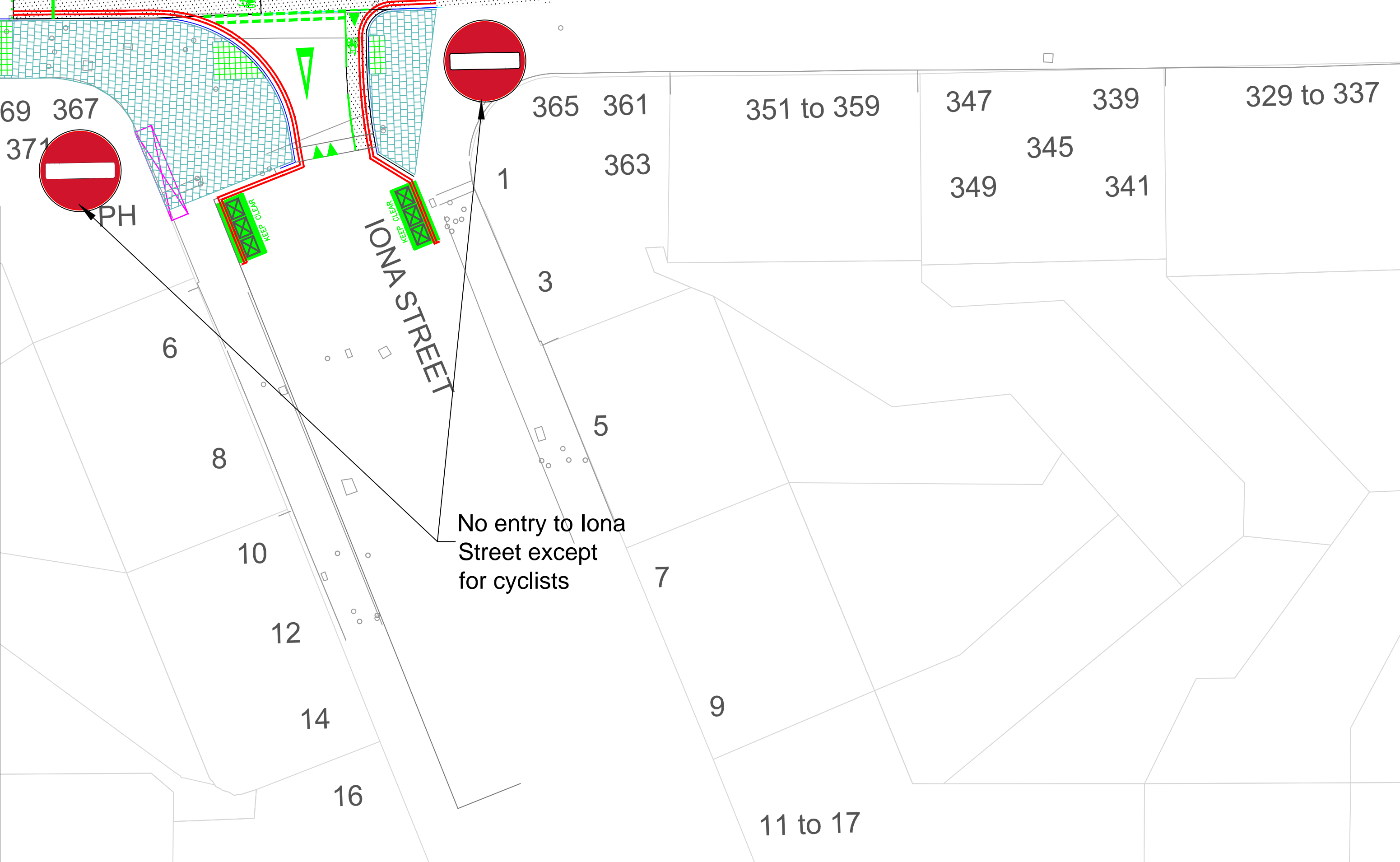
Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
 60 minutes maximum stay, no return within 90 minutes  
 No maximum stay for blue badge holders  
 Loading permitted

**Other times**  
 No restrictions

**Greenway loading bay restrictions**  
 No Stopping  
 Mon - Fri 7.30am - 9.30am and 4.00pm - 6.30pm

Mon - Fri 9.30am - 4.00pm and Saturday 8.30am - 6.30pm  
 No stopping except for loading or blue badge holders  
 60 minutes maximum stay, no return within 90 minutes for blue badge holders

**Other times**  
 No restriction



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A	Waiting/loading restrictions amended - Pilrig Street	AP	8/12/15	CS
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THE LEITH PROGRAMME  
 LEITH WALK  
 BRUNSWICK STREET TO DALMENY STREET  
 TRAFFIC REGULATION ORDER  
 APPENDIX 1 - STREET 2 OF 2

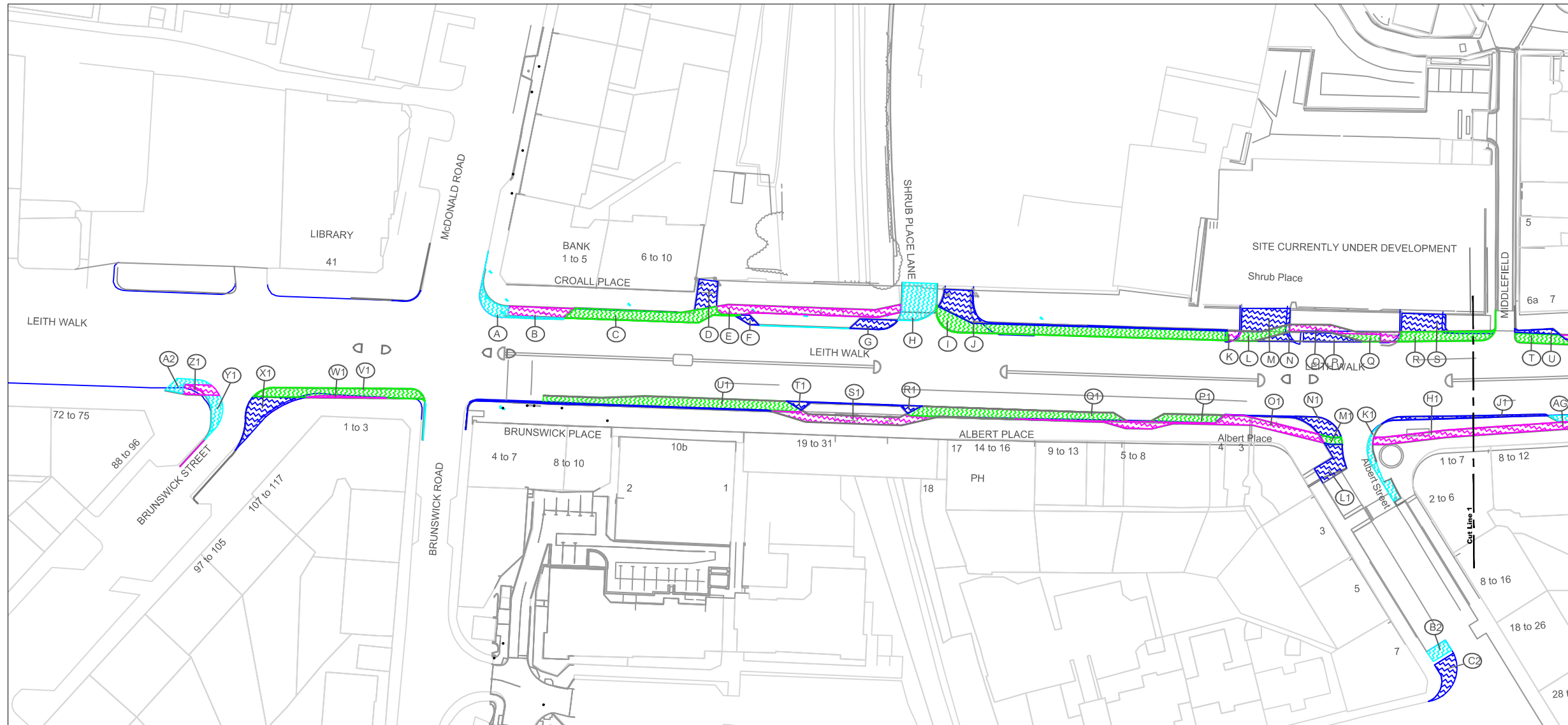
Date: July 2015 Job. No. 636045 Drawn by: S MACKENZIE  
 Scale: 1:250 Checked by: A Easson



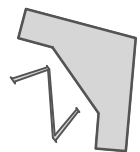
NOTES:

Legend

-  Existing carriageway redetermined as footway
-  Existing footway redetermined as carriageway
-  Existing carriageway redetermined as cycle track
-  Existing footway redetermined as cycle track



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Edinburgh, EH8 8BG  
Telephone: 0131 200 2000





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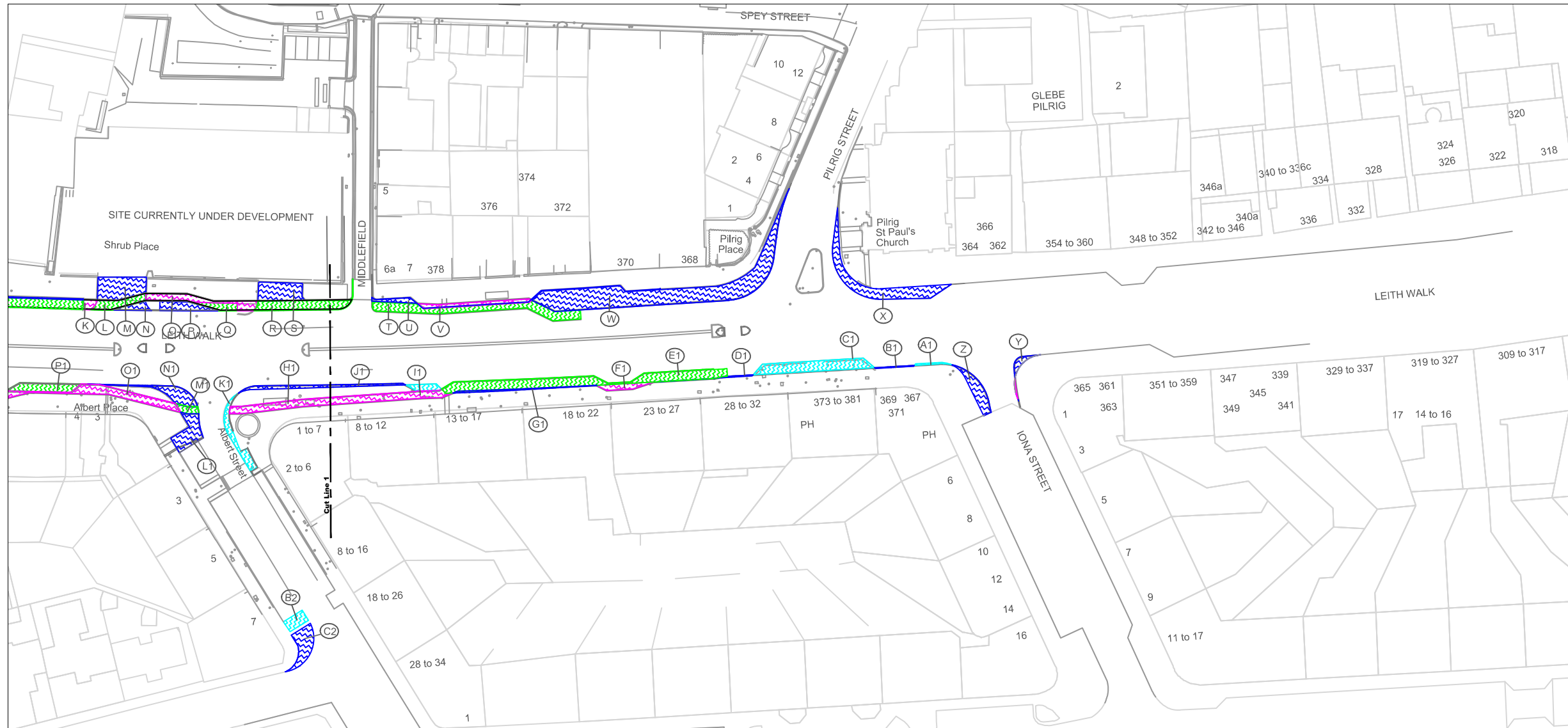
THE LEITH PROGRAMME  
LEITH WALK  
BRUNSWICK STREET TO IONA STREET  
RE-DETERMINATION  
SHEET 1 OF 2

Date: July 2015	Job No. 635045	Drawn by: S Mackenzie
Scale: 1:1000		Checked by: D Wilson

NOTES:

Legend

-  Existing carriageway redetermined as footway
-  Existing footway redetermined as carriageway
-  Existing carriageway redetermined as cycle track
-  Existing footway redetermined as cycle track



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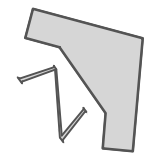


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Edinburgh, EH8 8BG  
Telephone: 0131 200 2000

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THE LEITH PROGRAMME  
LEITH WALK  
IONA STREET TO BRUNSWICK STREET  
RE-DETERMINATION  
DRAWING 2 OF 2

Date: July 2015 Job No. 635045 Drawn by: S Mackenzie  
Scale: 1:1000 Checked by: D Wilson



Drawing Name: G:\CD\Trans\Projects\Eng\635045 Leith Walk Train Reinstalls\TMO Drawings\Phase 4\Leith Walk phase 4 RSO Sheet 2.dwg

**Request Type - General Enquiry**

**Service Request Number - 887043**

**SfC\City Centre/Leith Neighbourhood Management**

**Received Date - 15/10/2015**

**Service Area - CELO - Partnership and Information**

**Target Date - 29/10/2015**

**Sub Service Area -**

**Response -**

**If Other -**

**Type - General Enquiry BO**

**Assigned to - alan.dean@edinburgh.gov.uk**

**Multiple Request Co Ordinator -**

**Street Name  
Ward**

**Neighbourhood  
Other System Refs**

**Subject** TRO/Phase4/note

**Summary** From: [REDACTED]  
Sent: 15 October 2015 11:52  
To: Traffic Orders  
Subject: TRO/Phase4/note

Dear Iain Buchanan

I just received a letter through the post about the proposed Traffic Regulations. I welcome the majority of the changes as I feel it will improve the area and we need that.

I have one major concern - the 'no entry restriction to Iona Street from Leith Walk.  
I live on South Sloan Street between Iona and Albert Street. Albert Street is already SUCH a mess - the street itself is in bad shape with potholes and bumps, the double parking-mostly white vans in the half closer to Leith walk, the speed at which people drive. There are many children and elderly people living along Albert Street and the impact of Iona street not being accessed via Leith Walk will lead to far greater numbers of cars using Albert Street. This is an impact Albert cannot cope with. It's far narrower than Iona Street and the double parking as well as the 'islands' that stick out mean that cars are already cueing to let each other by.

I have a great concern for the impact of the Iona Street restriction. Please reconsider this and let me know how this will be addressed.

[REDACTED]  
[REDACTED]  
\_\_\_\_\_  
[REDACTED]  
[REDACTED]  
[REDACTED]  
[REDACTED]  
[REDACTED]

**SR Closure Details**

**Date Closed**

**Resolution**

**SR Customers Information**

Customer Name	Phone	Mobile	Cust Type
[REDACTED]			Individual
[REDACTED]			
[REDACTED]			

**Activity History**

15 October 2015

Activity ID	Date Logged	Staff Name	Cust Name	Type of Contact
5395265	15/10/2015 12:24:14	Avril Kirkwood	[REDACTED]	Email - Inbound
<b>Comment</b>	From: [REDACTED] Sent: 15 October 2015 11:52 To: Traffic Orders Subject: TRO/Phase4/note			

Dear Iain Buchanan

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I have a great concern for the impact of the Iona Street restriction. Please reconsider this and let me know how this will be addressed.

All the best

[REDACTED]

#### Follow Up Details

ID	Date	Staff Name	Category
970519	15/10/2015	Avril Kirkwood	Back Office Update
<b>Details</b>	Assignment email to Alan Dean		
970435	15/10/2015	Avril Kirkwood	Assignment
<b>Details</b>	Assignment Email to environment-ccl@edinburgh.gov.uk		
970434	15/10/2015	Avril Kirkwood	Back Office Update
<b>Details</b>	Assignment email to environment-ccl		

#### Related SRs

Subject:

RE: Leith TRO/15/29

---

**From:** [REDACTED]  
**Sent:** 28 October 2015 16:24  
**To:** Traffic Orders  
**Subject:** Leith TRO/15/29

Dear Sir

The order number is TRO/15/29

I have been informed that the full order title is LEITH TRO/15/29

My Name and Address: [REDACTED]

I live in Albert Street. This is a residential street with a few small businesses operating at the junction with the commercial Street of Leith Walk.

I object strongly to the plan to introduce parking restrictions in Albert street. This will directly and negatively affect myself and other residents in the Street and the area.

There are parking restrictions currently on Leith walk and whatever parking arrangements you choose for Leith Walk should not cause the residents of Albert St to loose parking and have even more pressure on the parking. Your plans improve traffic flow directly at the expense of residents.

The vehicles that feel the need to park in Albert St for business reasons for longer than an hour will just move further down the street.

Cars that currently pull over on the Walk for a short time will now be driving into and out of Albert street making a busy, smelly junction. And if they can't park, they too will move further down the street looking for space as an alternative.

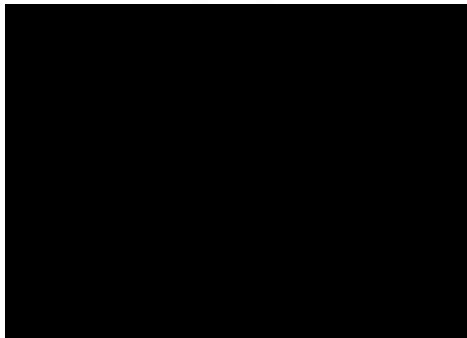
13 spaces is quite a lot and all the people who currently park there because they live there will have to find somewhere else to park, .....yes that's right,...further down the street.

For residents this action is draconian and downright mean and nasty. Especially in the light of the parking solution of resident permits that operate all over the city allowing visitors and residents to share the parking.

Please reconsider these plans that will turn residential life into a nightmare.

Yours sincerely

[REDACTED]



Head of Transport  
Services for Communities  
Waverley Court  
4 East Market Street  
Edinburgh  
EH8 8BG

Email: [trafficorders@edinburgh.gov.uk](mailto:trafficorders@edinburgh.gov.uk)

Date: 16 November 2015

Dear Sir

**Proposed Traffic Regulation Order and Redetermination Order – Leith Walk from Pilrig Street to McDonald Road (TRO/15/29, RSO/15/23)**

We write on behalf of [REDACTED] in reference to the letter received from Ian Buchannan advising of the proposals and amendments to the roads affected by proposed TRO and RSO above. We are specifically writing to object to the RSO aspect of the orders which propose to restrict vehicular access from Leith Walk into Iona Street.

By way of some background [REDACTED] is a family run business established in 1863 which operates a [REDACTED] building yard from Iona Street in Leith. The company has operated from this location since 1979 and with an annual turnover of £5million. This yard also acts as a supply yard for their other yard in Gorgie. The yard has an established list of around 300 account clients across a range of local businesses, local councils and large national construction companies. In addition to these accounts the yard handles, on average, in excess of 100 cash sales during a trading day.

The company is part of the National Buying Group supply chain meaning it is able to stock products from a range of external suppliers typically resulting in an average of 120 deliveries a month. In addition to this there are other suppliers delivering to the yard and of course the main timber deliveries to the yard from the company's Bo'Ness sawmill.

These deliveries can be made by a range in vehicles including large 16.5m vehicles. The yard also undertakes its own deliveries and has its own transport operator's licence and uses 2 Hi-Ab vehicles and 16.5m articulated vehicles.

In addition the Edinburgh operation employs 28 full time staff and supports the jobs of some 15 staff at the Bo'Ness sawmill.

As you will no doubt therefore appreciate the building yard is an established, successful family run business that supports a range of local companies in undertaking work within the Edinburgh area. It is crucial to the operation of the yard that it is accessible to both customers and suppliers and that access to the yard is available throughout the business operating hours.

[REDACTED]  
[REDACTED]  
[REDACTED]



The potential restriction of access into Iona Street from Leith Walk will mean that all traffic is routed from Easter Road or through other residential streets in the area. This would be a significant disruption to the operations to the extent that it would likely bring the future viability of the yard into serious question.

A recent survey of traffic to and from the yard showed that over the course of a week around 80% of all traffic to the yard travelled from Leith Walk.

The survey also showed that all large goods vehicle travelled to the yard to and from Leith Walk.

The information presented in support of the order states that "Traffic counts have been undertaken in order to establish the number of vehicles that currently enter Iona Street from Leith Walk and it is not considered likely that there will be an unacceptable impact on the various possible alternative routes by displaced". Whilst in principle the traffic volumes may be relatively small it is too simplistic to look at the traffic volumes without considering the nature or purpose of the traffic.

The geometry of the junction of Leith Walk with Iona Street is able to accommodate the large HGV vehicles associated with the timber yard operations without encroaching into the oncoming traffic. Compare this to the geometry of the Iona Street / Easter Road junction it is clear that in order to turn into and out of Iona Street large HGVs will require to occupy the entire road carriageway on both Easter Road and Iona Street (and we enclose Swept Path Assessments showing these manoeuvres). The surrounding residential roads are entirely inappropriate for use by large commercial vehicles, due to their tight geometry, high prevalence of parked cars and on occasion double parked vehicles meaning that Easter Road forms the only real alternative route.

Furthermore the design and layout of Iona Street beyond the yard access includes a number of raised tables which would require to be driven over by delivery vehicles with associated noise impacts for the residents of the tenement flats along that part of the road. The repeated requirement to drive over these vertical traffic humps will also undoubtedly have an impact on both the road carriageway (which is cobbled) and the vehicles themselves (we are aware for example that Lothian buses advise that they will not operate service through new developments with any vertical traffic calming features due to the potential for damage to their vehicles.)

The route to Easter Road also will require all our commercial related traffic to travel past the children's playground located to the north of Iona Street clearly an undesirable potential conflict which is currently able to be avoided.

For the avoidance of any doubt, our issue relates to the restriction of access from Leith Walk to Iona Street and the resultant impacts on both the viability of the ongoing business and the amenity on the remainder of Iona, specifically:

- Significant restriction and disruption to of access to the business resulting in a loss of trade;
- Safety concerns around increased HGV traffic travelling past the playground;
- Safety concerns around the physical ability of large HGVs to safely turn into and out of Iona Street from Easter Road; and
- The impact in terms of noise, road condition and vehicle maintenance as a result of having to travel over additional vertical traffic calming features.

We have no issue in principle to seeking to reduce the traffic volumes using Iona Street (with a knock one reduction in turning traffic on Leith Walk) but feel that the current proposals would result in significant impacts on our business and in fact make the residential / traffic calmed section of Iona Street busier with commercial traffic than it currently is. In our view the proposals would therefore whilst "creating a more attractive environment for pedestrians" on Leith Walk create a significantly less attractive and potentially less safe environment for pedestrians on both Iona Street and Easter Road.

Indeed we would suggest that if there is a desire to stop up Iona Street to through traffic it would be more appropriate to introduce such a restriction to the east of our business, which would maintain access from Leith Walk for our large HGV traffic. Whilst this would also provide some disruption to our access and business as traffic coming from Easter Road would no longer be able to directly access the yard from Iona Street this traffic is likely to be smaller vehicles more able to take alternative routes and represents a much smaller amount of our business.

We trust that the above is sufficiently clear and that you will give this representation due weight as you consider matters further. We look forward to hearing further from you as the proposals progress, but in the meantime please do not hesitate to contact me should you wish to discuss matters further.

[Redacted]

[Redacted]

[Redacted]

[Redacted]

[Redacted]

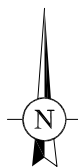
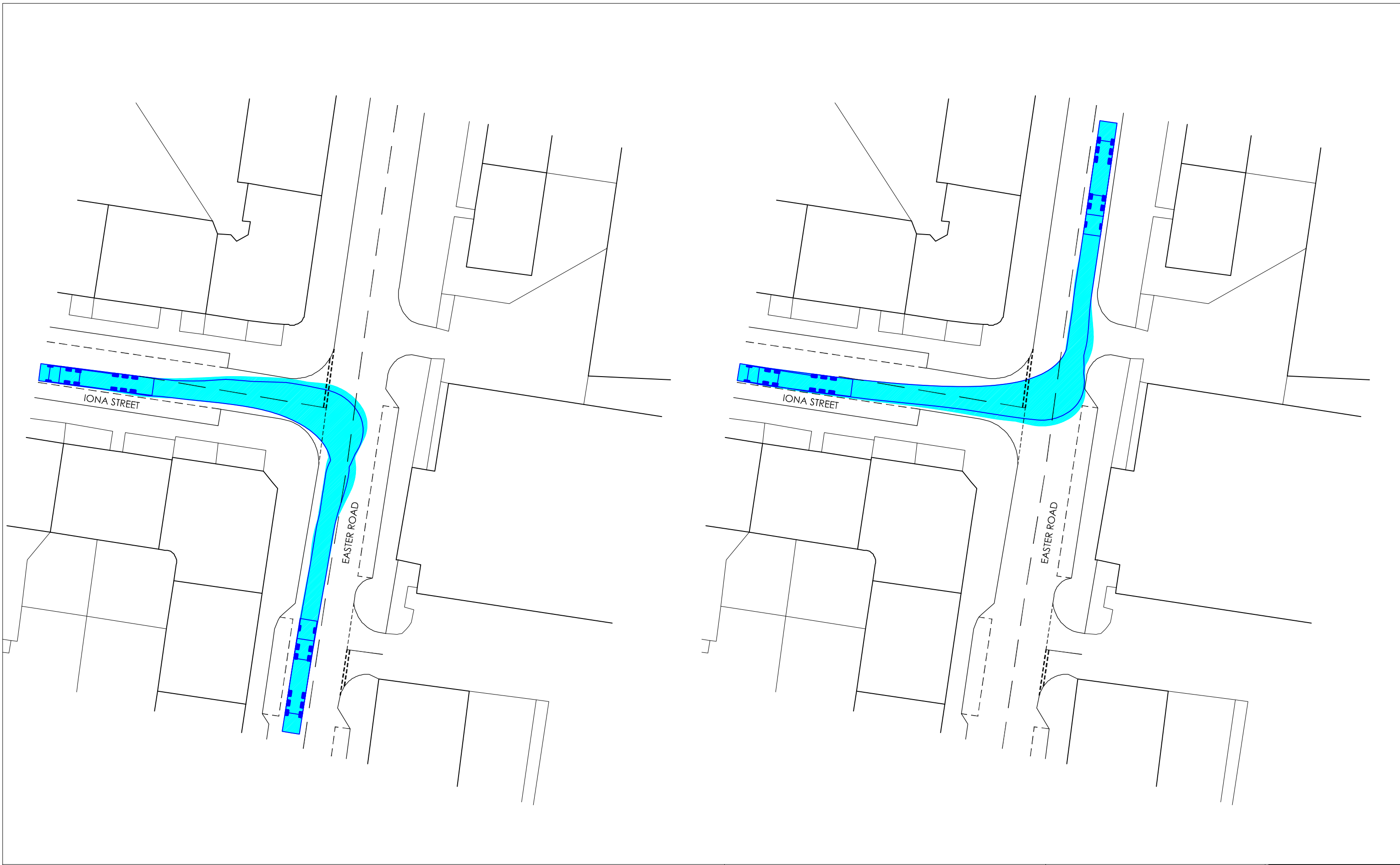
[Redacted]

Encs.

C.C.

[Redacted]

[Redacted]



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Iona Street, Edinburgh

Swept Path Analysis  
 16.5m HGV Turning in to Iona Street  
 from Easter Road

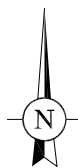
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 SP621/SK/201A

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Drawn by:  
 NW

Date:  
 Nov 2015

Checked by:  
 AS



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Iona Street, Edinburgh

Swept Path Analysis  
 16.5m HGV Turning out of Iona Street  
 on to Easter Road

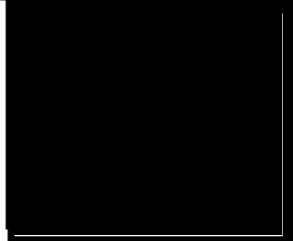
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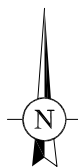
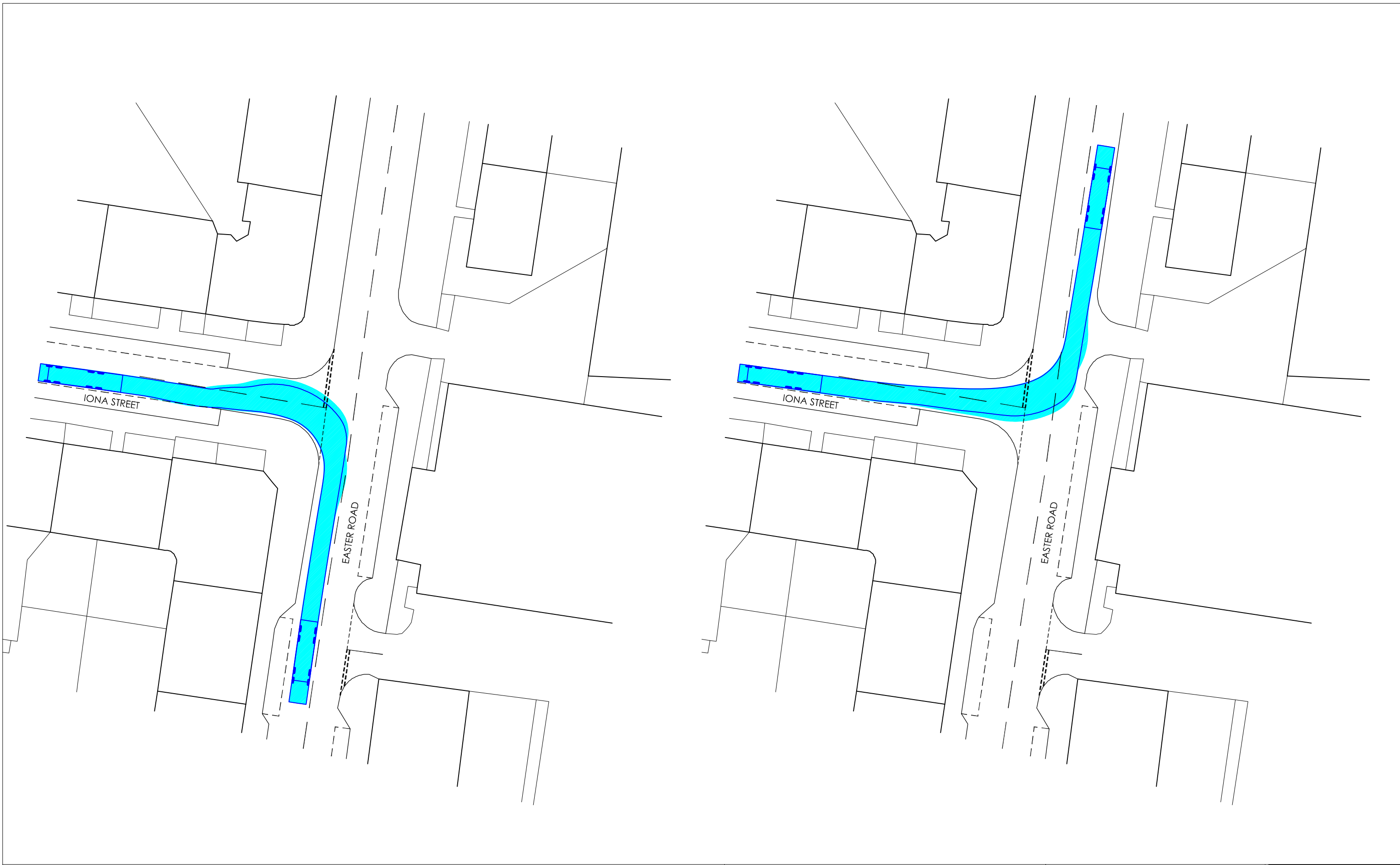
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 NW

Scale:  
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Iona Street, Edinburgh

Swept Path Analysis  
 12m Rigid Turning on to Iona Street  
 from Easter Road

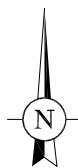
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 SP621/SK/203A

Drawn by:  
 NW

Scale:  
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Iona Street, Edinburgh

Swept Path Analysis  
 12m Rigid Turning out of Iona Street  
 on to Easter Road

Drawing Number:  
 SP621/SK/204A

Scale:  
 1:500 @ A3

Drawn by:  
 NW

Date:  
 Nov 2015

Checked by:  
 AS

TRO/15/29 The City of Edinburgh Council (Leith Walk Pilrig Street to McDonald Road Edinburgh) (Prohibition of Entry) and (Loading and Unloading Places) and (Various Streets, Edinburgh)(Prohibition and Restriction on Waiting) and (Disabled Parking Places) and (Various Roads, Edinburgh) (Prohibition of Waiting at Junctions) and (Greenways) (Variation), Order 201

RSO/15/23 The City of Edinburgh Council (Leith Walk Edinburgh) Redetermination of Means of Exercise of Public Right of Passage Order 201

Objection by:

[REDACTED]

[REDACTED]

[REDACTED]

[REDACTED]



16th November 2015

The Head of Transport Services for Communities,  
Edinburgh City Council

Dear Sir

THE CITY OF EDINBURGH COUNCIL ORDERS TRO 15/29 and RSO 15/23.

Although we support in principle the provision of cycle lanes in the section of Leith Walk between Pilrig Street and Macdonald Road; we object strongly to the City Council making these two Orders. Our objections are because the City Council has been too single-minded in these proposals for cycle lanes and has not taken sufficient account of the effects of the proposals on residents, pedestrians and bus passengers.

There are several concerns with the City Council proposals, but our main concern is with the changes at the junction of Leith Walk and Pilrig Street and the resulting restrictions proposed. We object strongly to these proposed changes and restrictions for the reasons given below.

The left turn lane into Pilrig Street from Leith Walk has been re-determined as footway. This has five effects: it halves the number of traffic lanes on the northbound Leith Walk approach; it precludes the current pedestrian crossing arrangement which at present permits full pedestrian movements that do not delay vehicular flows; it produces a sharp cutting movement of the traffic turning left into Pilrig Street across the proposed cycle lane; it removes the bus stop from outside the shops; and it moves the unloading bay from outside the shops to the traffic side of the cycle lane.

In the City Council proposals all pedestrian crossing movements would have to occur simultaneously. All traffic movements would need to be halted during this time. The time available for pedestrians to cross Leith Walk would need to be reduced by between a half and a third and the maximum capacity of the junction would be reduced by about 20%. Basic laws of maths and physics dictate that there would be additional delays and pollution as a result.

The northbound Leith Walk flow would suffer from both the pedestrian crossing time and from being reduced to a single lane. Additional peak hour delays and the loss of the left turn lane would encourage rat-runs through Spey Terrace and Dryden Street.

The City Council proposals extend the cycle lane alongside the kerb until the back of a cyclist reservoir. This is standard at many junctions but not where there is a substantial left-turn flow. Taking the cycle lane along the kerb results in a sharp cutting movement by significant volumes of traffic turning left across the cyclists. This is not wise. The standard solution is to provide separate lanes for left turning traffic and ahead traffic with a cycle lane between the two lanes providing access to a cyclist reservoir. You have utilised this concept at the Milton Road East/ Eastfield junction on the A199 so you will be familiar with the benefits of its use.



Cyclists are permitted to turn right from Leith Walk into Iona Street but no protecting islands are being provided. There may be no alternative – unless cyclists use the pedestrian crossing across Leith Walk – but it looks like hanging cyclists out to dry.

Bus services are intended to be convenient and attractive to their users. The loss of the bus stop at Scotmid is inconvenient as we use the stop on a regular basis to shop on journeys back from town. The proposed replacement bus stop in Pilrig Street is inappropriate. It doesn't serve the shops and it is at the narrowest point on the footway where no shelter could be provided. The narrow footway is difficult enough for pedestrians – in particular when its recycling day - and we object to the additional obstruction to pedestrians that the proposed bus stop would create and to the inconvenient change to the bus stop provision.

The proposed position of the replacement bus stop in Pilrig Street results in the City Council proposing parking and loading restrictions across three houses on the even side of the road and a block of five terraced houses on the odd side of the road. The restrictions across the block of five terraced houses on the odd side of the road are drafted as applying for 24 hours every day of the year "to avoid congestion". That is absurd. No-one can seriously suggest that a car parked at 2 in the morning or 10 at night in front of these properties would cause congestion. The proposed provision is thoughtless and arrogant. The bus stop will just be a request stop that would see very little use even in peak periods.

Attached is a suggested alternative layout for Leith Walk that includes the standard cycle lane layout for use where there is significant left-turning traffic. It permits the retention of the number eleven bus-stop by the shops in Leith Walk; it allows for an off-peak loading / unloading bay at those shops; it allows for retention of the current pedestrian crossing facilities and pedestrian crossing times; and it retains the capacity of the junction.

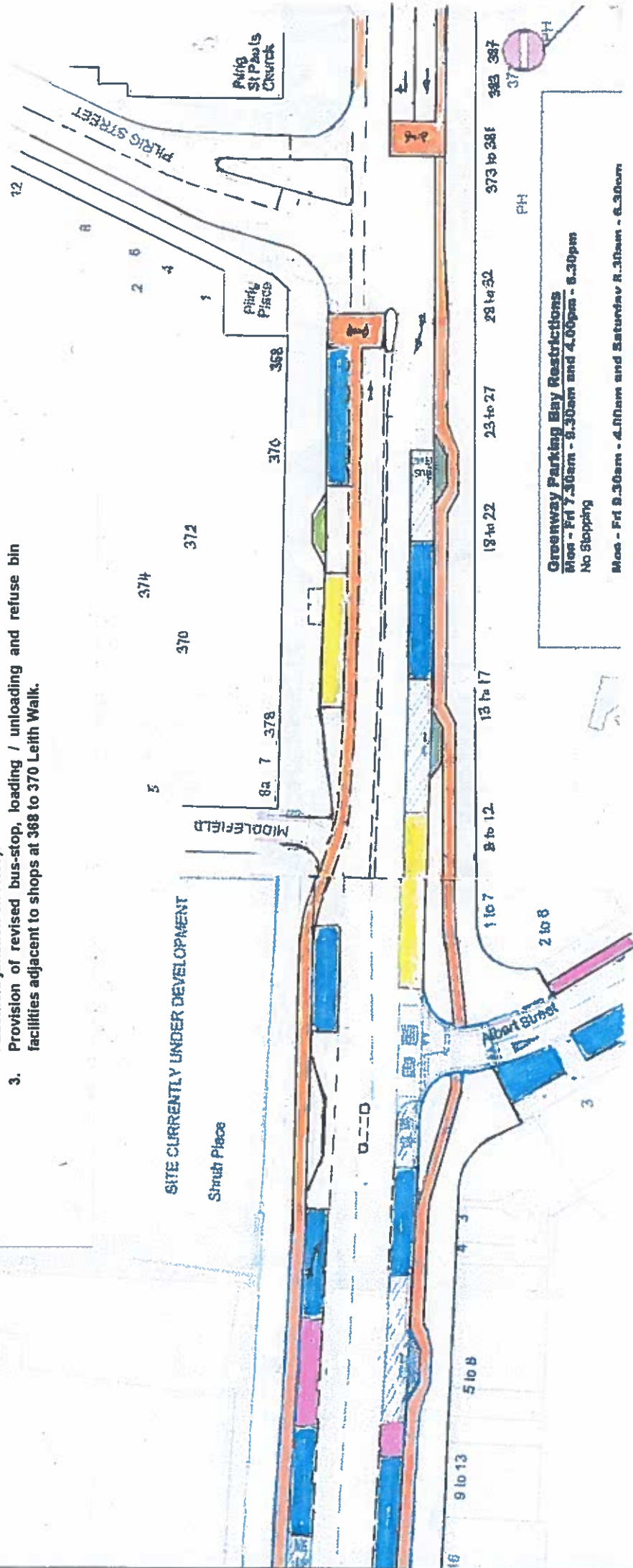
This alternative layout fits to the same standards as used elsewhere in your proposals or in your recent work in Leith Walk and uses the same colour coding for bus, parking, loading / unloading and refuse bin facilities as in the Appendix the the Council's draft TRO. It would be beneficial if the pedestrian area from 1 Pilrig Place up to Leith Walk could be widened and the pedestrian area alongside the church narrowed to compensate if this is within your budget. This has been shown on this alternative layout but it is not essential to its functioning.

Your sincerely



Proposed Amendment to TRO 16/29

1. Provision of a build-out for pedestrians at the new Albert Street crossing point of Leith Walk on the Shrub Place side to assist pedestrian safety and to protect cyclists entering the cycle lane.
2. Provision of standard junction protecting ahead cycle movements where there is significant left-turn traffic flow. (As per Milton Road East / Eastfield junction on A198).
3. Provision of revised bus-stop, loading / unloading and refuse bin facilities adjacent to shops at 388 to 370 Leith Walk.



**Greenway Parking Bay Restrictions**  
 Mon - Fri 7.30am - 8.30am and 4.00pm - 6.30pm  
 No Stopping

Mon - Fri 8.30am - 4.00am and Saturday 8.30am - 6.30am

TRAFFIC ORDERS TRO 115/29

RSO 115/23

THE HEAD OF TRANSPORT SERVICES FOR COMMUNITIES

Transport,  
Room 10/19,  
CITY CHAMBERS

CITY CHAMBERS

HIGH STREET

EDINBURGH

RECEIVED  
16 NOV 2015

TAO 115/29 + RSO 115/23

BOTH

Date 9 December 2015

Your ref

Our ref TP/01/002/2/CS

Dear [REDACTED]

**OBJECTION TO TRO/15/29 – LEITH WALK  
BRUNSWICK STREET TO DALMENY STREET**

Thank you for your e-mail of 15 October 2015 stating your objection to the above Traffic Regulation Order. Please find some information below which relates to your objection.

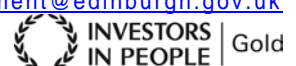
It is proposed to prohibit entry into Iona Street from Leith Walk for vehicles in order to improve traffic flow in the area around the Pilrig Street and Iona Street junctions, mainly by preventing queues of traffic waiting to turn right into Iona Street from obstructing the Leith Walk/Pilrig Street junction.

The prohibition of this right turn was suggested by Leith Central Community Council during an earlier Leith Programme consultation however it will be possible to provide a clearer and more self-enforcing road layout by prohibiting entry for all vehicles rather than by only prohibiting right turn manoeuvres.

In line with commitments in the Council's Active Travel Action Plan, cyclists will be exempted from the proposed prohibition on entry. A contra-flow cycle lane will be provided to allow cycle access into Iona Street from Leith Walk.

Traffic counts were carried out as part of the design process to establish the number of vehicles currently entering Iona Street from Leith Walk. While restricting access to Iona Street will result in additional traffic using adjoining streets including Albert Street, this traffic is likely to be displaced onto a number of possible alternative routes, some nearby and some further afield as part of a more strategic re-routeing. As such, the Council does not expect displacement of traffic to cause a significant problem on any individual alternative route.

**Callum Smith, Senior Professional Officer, (Projects Development), Place**  
Planning and Transport, C2, Waverley Court, 4 East Market Street, Edinburgh EH8 8BG  
Tel 0131 469 3592 Fax 0131 529 6201 [transport.projectsdevelopment@edinburgh.gov.uk](mailto:transport.projectsdevelopment@edinburgh.gov.uk)



Should the information provided be sufficient to allow you to withdraw your objection to the advertised Order, please could you reply in writing (letter or e-mail) within 14 days of receipt of this letter. If we do not hear from you within this period, it will be assumed that you wish to maintain your objection.

A report on the maintained objections relating to the Traffic Regulation Order will be made to the Council's Transport and Environment Committee on 12 January 2016. The report will be available to view on the Council's website seven days prior to the Committee meeting - this can be viewed at: <http://www.edinburgh.gov.uk/cpol>.

Should you wish to discuss this matter further, please do not hesitate to contact me using the details provided.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Callum Smith', written in a cursive style.

**Callum Smith**  
**Senior Professional Officer**  
**(Projects Development)**

Date 9 December 2015

Your ref

Our ref TP/01/002/2/CS

Dear [REDACTED]

**OBJECTION TO TRO/15/29 – LEITH WALK  
BRUNSWICK STREET TO DALMENY STREET**

Thank you for your e-mail of 28 October 2015 stating your objection to the above Traffic Regulation Order. Please find some information below which relates to your objection.

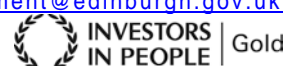
The Council appreciates that it is important for businesses and residents to have access to parking and loading facilities, and has sought to ensure that these are provided at the most suitable locations to meet local demand.

It is proposed to convert 22 currently uncontrolled parking spaces on the short section of Albert Street between Leith Walk and Murano Place into three loading spaces and 19 short stay parking spaces. There are no proposals to make any changes to the uncontrolled parking on the remainder of Albert Street east of Murano Place.

The proposed short stay bays can be used up to a maximum of 60 minutes between 7.30am and 6.30pm (Monday to Friday). There is no maximum length of stay at all other times. This means that residents will be able to use these bays for evening and overnight parking between 5.30pm and 8.30am on weekdays and at all times at weekends.

The short stay bays are proposed to offset the loss of peak period parking and loading facilities and short stay parking bays on Leith Walk. The 60 minute limit will increase the turnover of these spaces to the benefit of local businesses and customers looking for short stay parking. The bays will also be consistent with the 60 minute maximum stay in force for parking bays on Leith Walk on Mondays to Saturdays.

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Should the information provided be sufficient to allow you to withdraw your objection to the advertised Order, please could you reply in writing (letter or e-mail) within 14 days of receipt of this letter. If we do not hear from you within this period, it will be assumed that you wish to maintain your objection.

A report on the maintained objections relating to the Traffic Regulation Order will be made to the Council's Transport and Environment Committee on 12 January 2016. The report will be available to view on the Council's website seven days prior to the Committee meeting - this can be viewed at: <http://www.edinburgh.gov.uk/cpol>.

Should you wish to discuss this matter further, please do not hesitate to contact me using the details provided.

Yours sincerely



**Callum Smith**  
**Senior Professional Officer**  
**(Projects Development)**



[REDACTED]  
[REDACTED]  
[REDACTED]

Date 9 December 2015

Your ref

Our ref TP/01/002/2/CS

Dear [REDACTED]

**OBJECTION TO TRO/15/29 – LEITH WALK (BRUNSWICK STREET TO DALMENY STREET) AND RSO/15/23 – LEITH WALK (BRUNSWICK STREET TO IONA STREET)**

Thank you for your letter of 16 November 2015 stating your objection to the above Traffic Regulation Order and Redetermination Order. Please find some information below which relates to your objection.

### **Significant Restriction and Disruption of Access**

The proposed changes would not prevent vehicles leaving the yard from joining Leith Walk, while traffic entering or exiting the yard from Easter Road would also be unaffected. As such, three of the four currently possible movements to or from the business would be unaffected by the proposed changes. It is therefore considered that there are a number of suitable alternative routes to/from the business.

### **Increased HGV Traffic Travelling Past Play Park**

The play area within Dalmeny Street Park cannot be directly accessed by pedestrians from Iona Street, and is securely fenced off. As such, there are no safety concerns in relation to the play park arising from the proposed changes at the Leith Walk/Iona Street junction.

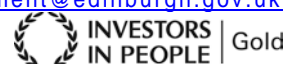
### **Ability of HGV Traffic Entering Iona Street from Easter Road**

Thank you for providing swept-path analyses with your objection – these have been reviewed whilst considering the points raised.

For large goods vehicles entering Iona Street from Easter Road, it is noted that the vehicle would be required to use a short length of the opposite side of the carriageway on Iona Street. However, this is a common occurrence at side road junctions across the city and, as such, this does not give rise to any particular safety concerns.

We have carried out a comparable swept-path analysis for the same vehicle types (12 metre rigid vehicle and 16.5 metre heavy goods vehicle) at the Leith Walk/Iona Street junction, and this has confirmed that these vehicles are required to use the opposite side of Iona Street at this junction at present. Plans showing this analysis are enclosed.

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It is also noted that, for a 16.5 metre heavy goods vehicle entering Iona Street from Easter Road, it may be necessary for the vehicle to use a very short section of the opposite side of the carriageway on Easter Road itself before entering Iona Street. However again this is a common recurrence and would simply require the vehicle to wait for a gap in oncoming traffic.

I note that you also supplied swept path analyses for vehicles exiting onto Easter Road from Iona Street but would note that the proposed restrictions would not force vehicles to undertake this manoeuvre, as exiting Iona Street onto Leith Walk would still be permitted.

### **Vertical Traffic Calming Features on Iona Street**

The vertical traffic calming measures which are in place on Iona Street are standard features which are common across the city and should not cause any issues for vehicles driving to the road conditions, including large goods vehicles

### **Alternative Suggestion**

An alternative suggestion to stop up Iona Street east of your client's business was included within the objection, which would maintain access to the west end of Iona Street for vehicles from Leith Walk. However, the proposal to prohibit entry at the Leith Walk junction does not seek to reduce traffic levels on Iona Street but is proposed in order to improve traffic flow in the area around the Pilrig Street and Iona Street junctions, mainly by preventing queues of traffic waiting to turn right from obstructing the Leith Walk/Pilrig Street junction.

The prohibition of this right turn was suggested by Leith Central Community Council during an earlier Leith Programme consultation, however it will be possible to provide a clearer and more self-enforcing road layout by prohibiting entry for all vehicles rather than by only prohibiting right turn manoeuvres.

### **Next Steps**

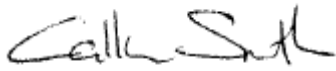
I note that your objection appears to relate entirely to issues concerning the proposed restriction on vehicle access from Leith Walk onto Iona Street, which form part of the Traffic Regulation Order rather than the Redetermination Order. However, I can confirm that we are currently considering your submission as an objection to both the Traffic Regulation Order and Redetermination Order processes.

Should the information provided be sufficient to allow you to withdraw your objection to either of both of the two statutory processes, please could you reply in writing (letter or email) within 14 days of receipt of this letter. If we do not hear from you within this period, it will be assumed that you wish to maintain your objection to both Orders.

A report on the maintained objections relating to the Traffic Regulation Order and Redetermination Order will be made to the Council's Transport and Environment Committee on 12 January 2016. The report will be available to view on the Council's website seven days prior to the Committee meeting - this can be viewed at: <http://www.edinburgh.gov.uk/cpol>.

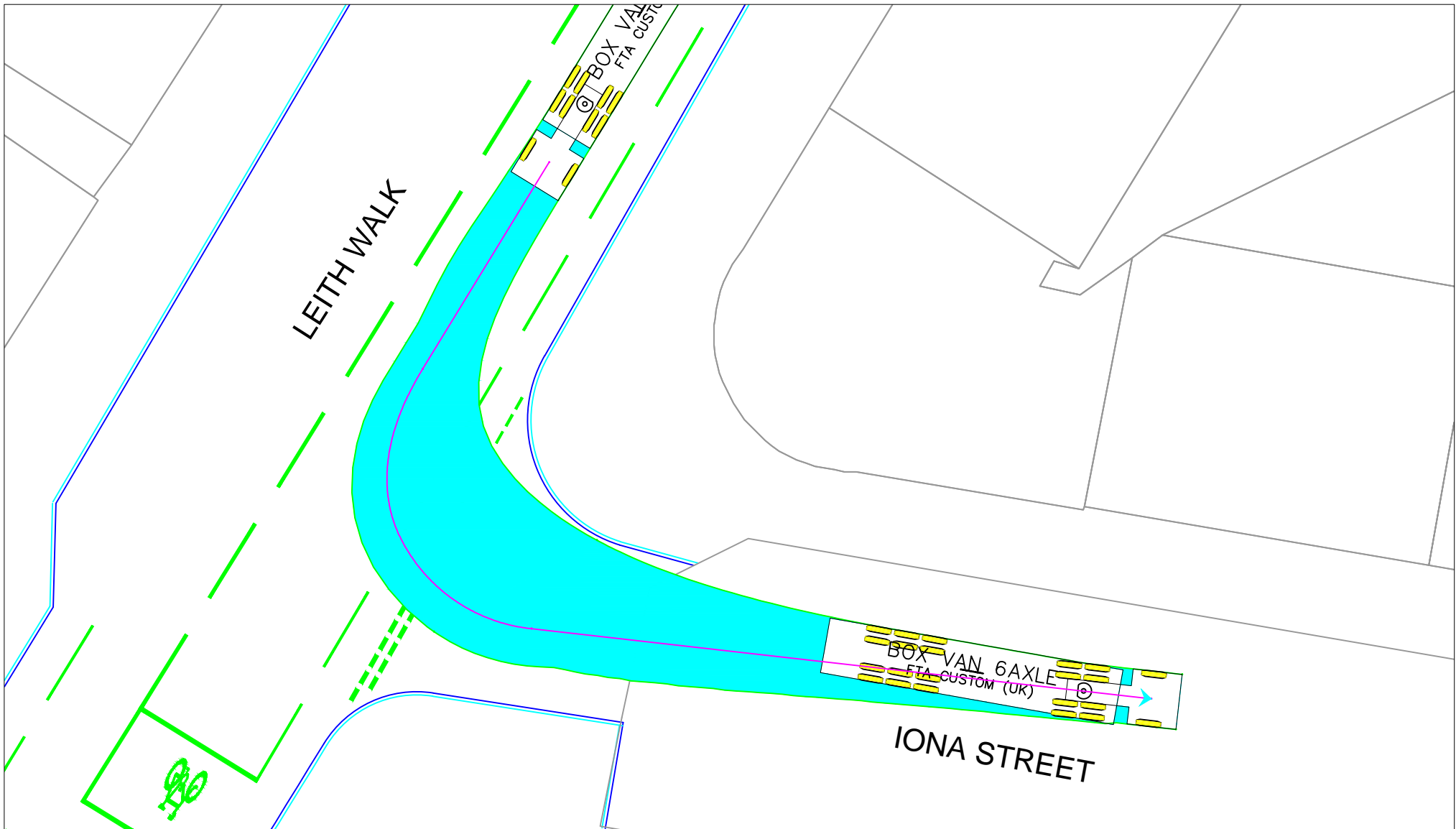
Should you wish to discuss this matter further, please do not hesitate to contact me using the details provided.

Yours sincerely

A handwritten signature in black ink, appearing to read 'Callum Smith', written in a cursive style.

**Callum Smith**  
**Senior Professional Officer**  
**(Projects Development)**

Encs



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THE CITY OF EDINBURGH COUNCIL

SERVICES FOR COMMUNITIES  
The City of Edinburgh Council  
Waverley Court, 4 East Market Street  
Edinburgh, EH8 8BG  
Telephone: 0131 200 2000

TRANSPORT  
TRAFFIC & ENGINEERING  
Transport Design & Delivery

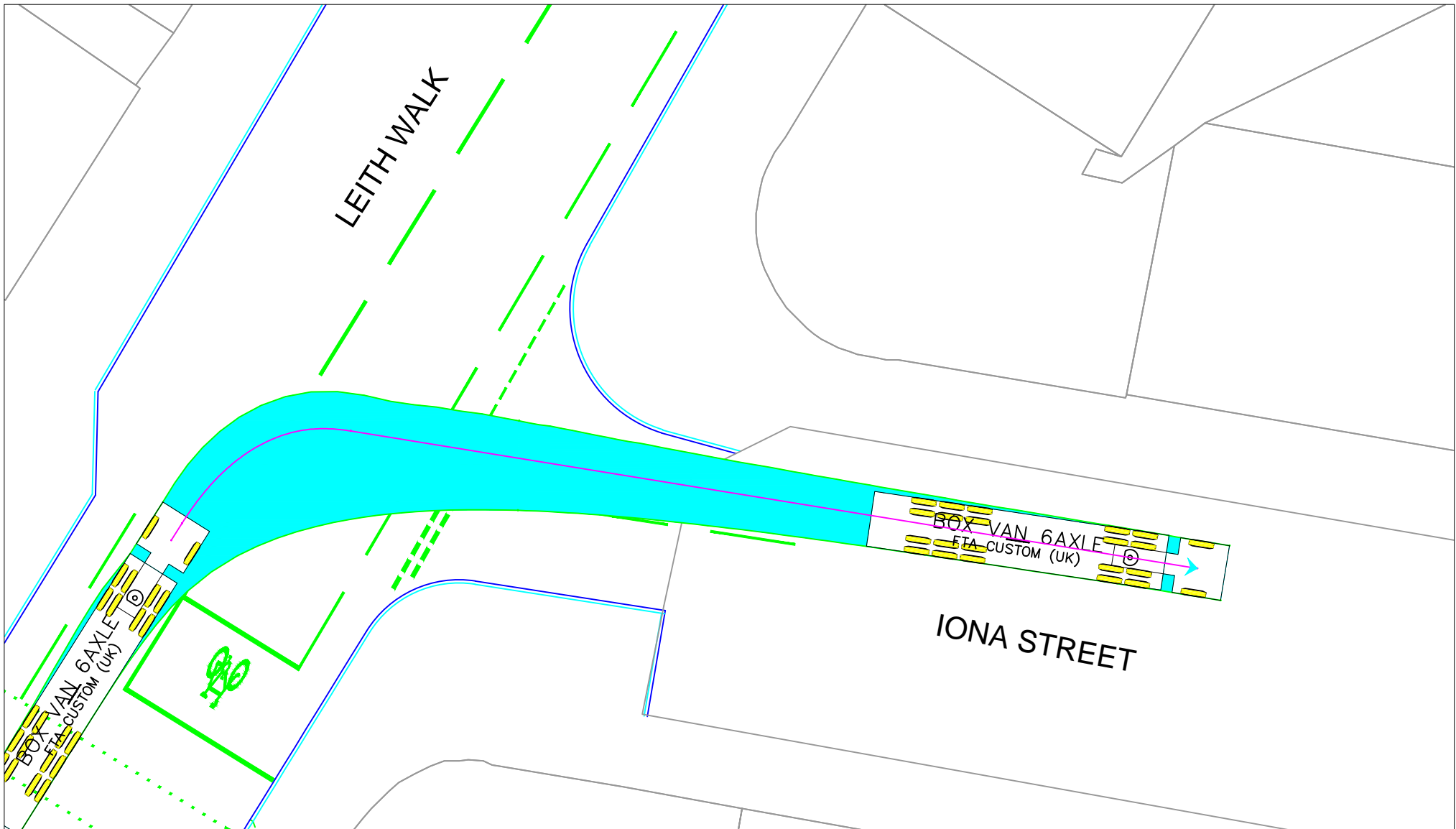
Leith Walk / Iona Street Junction  
16.5m Maximum Size Articulated Vehicle

Date: 9 December 2015  
Scale: 1:250

Drawn by: A Parkinson  
Checked by: A Easson

DRG NO. Sketch 1





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Leith Walk / Iona Street Junction  
16.5m Maximum Size Articulated Vehicle

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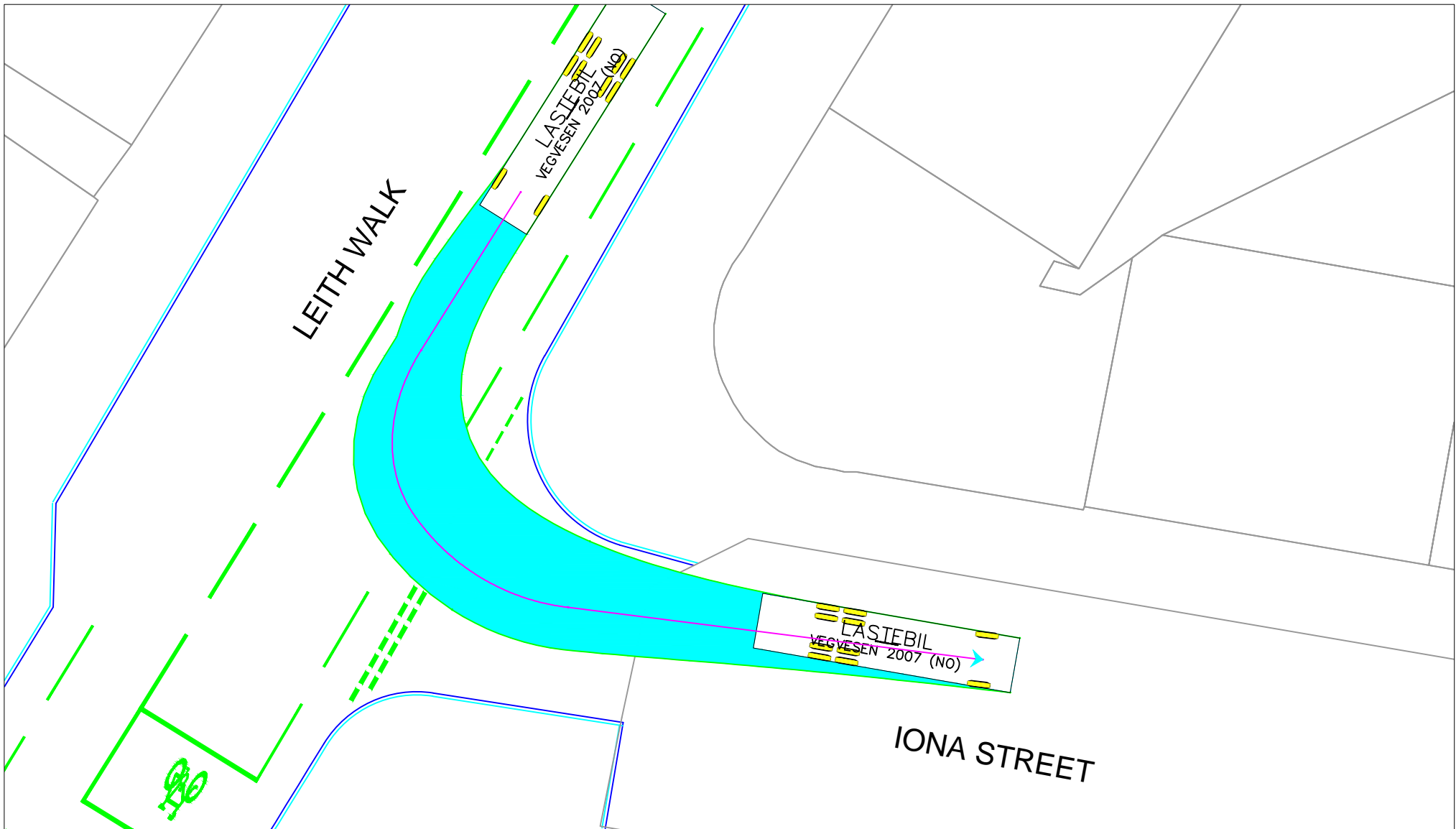
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Transport Design & Delivery

Date: 9 December 2015  
Scale: 1:250

Drawn by: A Parkinson  
Checked by: A Easson

DRG NO. Sketch 2





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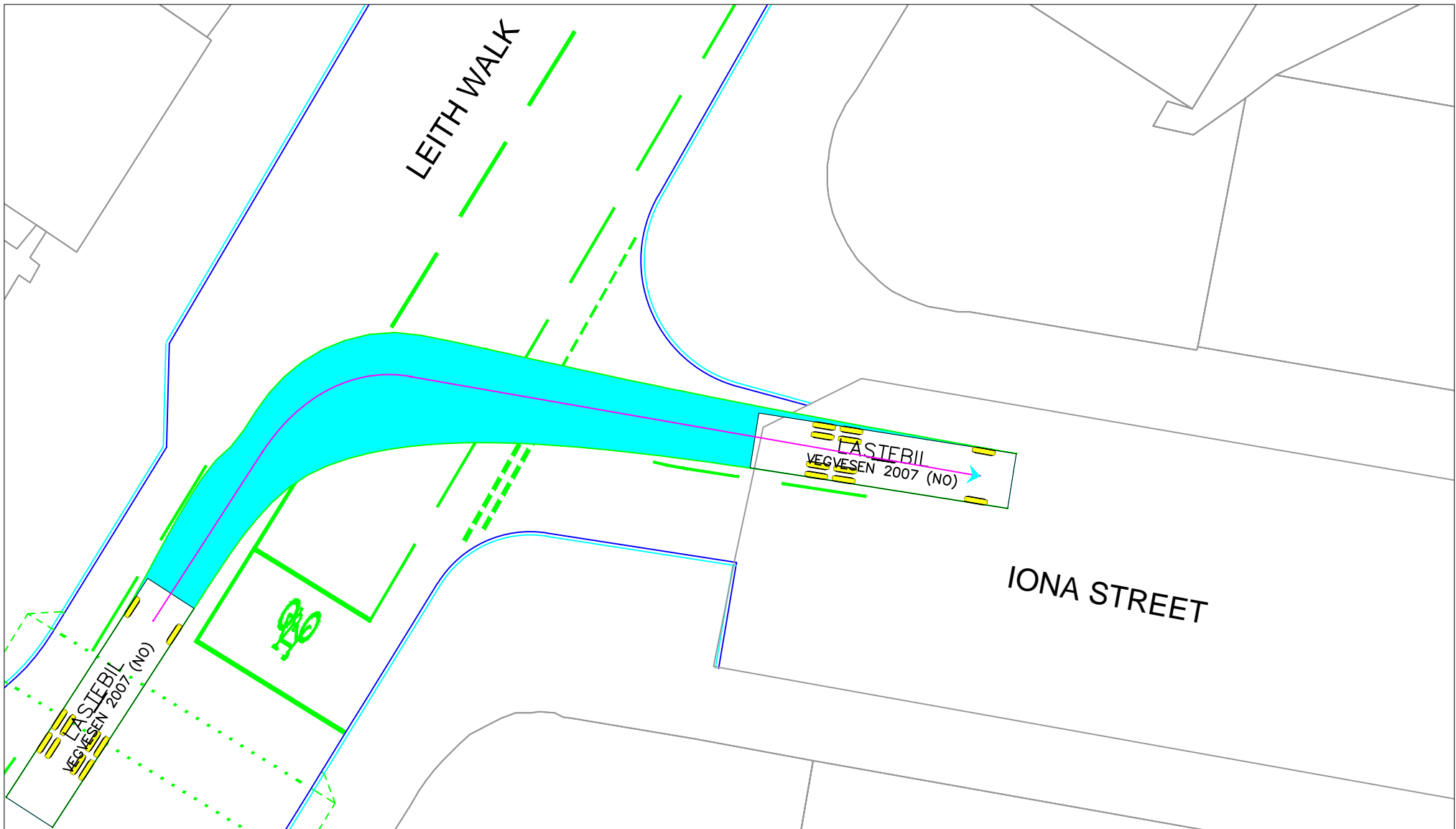
Leith Walk / Iona Street Junction  
12m Rigid Vehicle

Date: 9 December 2015  
Scale: 1:250

Drawn by: A Parkinson  
Checked by: A Easson

**DRG NO. Sketch 3**





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THE CITY OF EDINBURGH COUNCIL

Leith Walk / Iona Street Junction  
12m Rigid Vehicle

SERVICES FOR COMMUNITIES  
The City of Edinburgh Council  
Waverley Court, 4 East Market Street  
Edinburgh, EH8 8BG  
Telephone: 0131 200 2000

TRANSPORT  
TRAFFIC & ENGINEERING  
Transport Design & Delivery

Date: 9 December 2015  
Scale: 1:250

Drawn by: A Parkinson  
Checked by: A Easson

DRG NO. Sketch 4



Date 9 December 2015

Your ref

Our ref TP/01/002/2/CS

Dear [REDACTED]

**OBJECTION TO TRO/15/29 – LEITH WALK (BRUNSWICK STREET TO DALMENY STREET) and RS0/15/23 – LEITH WALK (BRUNSWICK STREET TO IONA STREET)**

Thank you for your letter of 16 November 2015 stating your objection to the above Traffic Regulation Order and Redetermination Order. Please find some information below which relates to your objection.

### **Leith Walk/Pilrig Street Junction**

The proposed changes at the Leith Walk/Pilrig Street junction reflect the principles of the Scottish Government's 'Designing Streets' guidance and are in keeping with the design principles for the Leith Programme as a whole, which were determined following extensive consultation with local stakeholders. These principles are consistent for all phases of the Programme, and promote simplified road layouts which are attractive for pedestrians and cyclists.

Under the proposals, to facilitate improvements for pedestrians and cyclists the northbound approach would be reduced from two traffic lanes at present to one lane, and the pedestrian crossing phasing at the junction would also be changed. There would be significant benefit for pedestrians as a result of widened footways and a single stage crossing on all legs of the junction. While journey times for vehicles are likely to increase, it is not considered that any unacceptable impacts will arise from the proposed changes in terms of congestion or that traffic will be displaced onto alternative routes.

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Tel 0131 469 3592 Fax 0131 529 6201 transport.projectsdevelopment@edinburgh.gov.uk



The layout on the northbound approach to the junction, whereby left turning traffic into Pilrig Street is required to cross the cycle lane, is a standard layout and no issues are anticipated. The Lothian cycling body SPOKES have been consulted on the proposals and have expressed support for the scheme in general, and I can confirm that they have raised no concerns in relation to the proposed junction layout.

Under the existing layout, cyclists are not only in conflict with vehicles crossing the cycle lane to reach the left-turn lane, but also with the bus stop and loading bay on the northbound approach to the junction. The proposed new layout would remove all conflicts with the bus stop at this location, and would reduce conflicts between vehicles using the loading bay, cyclists and left-turning traffic. Under the new layout, the issue with cyclists and left-turning traffic would be confined to a specific location rather than over a greater length as at present.

### **Relocation of Bus Stop from Leith Walk to Pilrig Street**

Relocating the bus stop from Leith Walk will remove potential for conflict between buses using the existing stop and cyclists and northbound traffic approaching the junction at Pilrig Street. The relocation will also allow pedestrian and cycling facilities to be improved on Leith Walk.

The stop is only used by one service (Lothian Buses No. 11), and Lothian Buses have been consulted over the proposed relocation and have no objections.

The changes to waiting and loading restrictions on Pilrig Street are proposed in order to ensure two way traffic flow past the new position of the stop. The main concern is that, if two way traffic past the new stop is not possible due to parked vehicles, westbound traffic will queue back to Leith Walk when a bus is stationary at the stop. However, it is accepted that this is unlikely to occur in the evenings or on Sundays. It is therefore proposed to change the double yellow line restrictions on the north side of Pilrig Street between Pilrig Glebe and Arthur Street to a single yellow line restriction (Monday to Friday 7.30am – 6.30pm, Saturday 8.30am – 6.30pm). This will enable parking and loading on the single yellow line overnight and on Sundays.

The number of passengers picked up by buses at the stop is very low, with the stop operating mainly as a drop off point. As such, no bus shelter is required on the footway, and there are expected to be extremely few occasions where waiting passengers may cause an obstruction. It is also proposed to widen the footway slightly to 1.8 metres, however it is not possible to increase this further due to width restraints on Pilrig Street. A temporary bus stop was also introduced at this location during previous tram works on Leith Walk, and this operated satisfactorily.



## Next Steps

I can confirm that we are considering your submission as an objection to both the Traffic Regulation Order and Redetermination Order processes.

Should the information provided be sufficient to allow you to withdraw your objection to either or both of the two statutory processes, please could you reply in writing (letter or e-mail) within 14 days of receipt of this letter. If we do not hear from you within this period, it will be assumed that you wish to maintain your objection to both Orders.

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Should you wish to discuss this matter further, please do not hesitate to contact me using the details provided.

Yours sincerely



**Callum Smith**  
**Senior Professional Officer**  
**(Projects Development)**

